

## ROADWAY IMPROVEMENTS

PROJECT NAME - SET DESCRIPTION

# INDEX OF SHEETS

SEE SHEET NO. 1A



## PROJECT BOUNDARY

END PROJECT (PRELIMINARY)  
STA. 108+24.30 COLLEGE STREET

**VICINITY MAP**  
NOT TO SCALE

**CITY OF MARYVILLE  
BLOUNT COUNTY, TENNESSEE**

**MAYOR: ANDY WHITE**

**CITY MANAGER: GREG MCCLAIN**

## PROJECT DESIGN TEAM

## CIVIL ENGINEER

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CONTACT: ROBERT LUSBY, P.L.S.



## OWNER / DEVELOPER



CITY OF MARYVILLE  
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MARYVILLE, TN, 37801  
PHONE: 865-273-3523

CONTACT: KEVIN STOLTENBERG

**PLANS PREPARED BY**

**Kimley»»Horn**

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PLAN REVISIONS			
REVISION NO.	DATE	REMARKS	
FILE NUMBER	SHEET NUMBER		TOTAL SHEETS
<b>118019006</b>	<b>1</b>		<b>40</b>

**ENGINEER'S SEAL**

**Know what's below.  
Call before you dig.**

Drawing name: K:\NSH\_Roadway\118019006 - Maryville Streetscape\Cadd\Plans\001 - Title Sheet.dwg      TITLE SHEET      Feb 14, 2024      1:23pm      by: Ben.Vondenbrink



## ROADWAY INDEX


# STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION	DWG.	REV.	DESCRIPTION
TITLE SHEET .....	1	STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS			CATCH BASINS AND MANHOLES		
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A	RD-TP-1	09-26-16	STANDARD ROADWAY DRAWINGS TITLE SHEET	D-CB-12P	02-20-20	STANDARD PRECAST RECTANGULAR CONCRETE NO.12 CATCH BASIN
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B - 2B2	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L	D-CB-12RB	02-20-20	STANDARD PRECAST 60" AND 72" CIRCULAR NO. 12 CATCH BASIN FOR USE WITH 6" VERTICAL CURB
GENERAL NOTES.....	2C	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z	D-CB-99	02-20-20	MISCELLANEOUS DETAILS FOR RECTANGULAR STRUCTURES
ENVIRONMENTAL NOTES.....	2E	RD-L-1	02-20-20	STANDARD LEGEND	D-CB-99R	01-28-22	MISCELLANEOUS DETAILS FOR ROUND STRUCTURES
TABULATED QUANTITIES .....	2F	RD-L-1A		STANDARD LEGEND	D-CB-99RA	10-29-21	BILL OF STEEL FOR ROUND CATCH BASIN LIDS
DETAIL SHEETS .....	2G - 2G2	RD-L-2	02-20-20	STANDARD LEGEND FOR UTILITY INSTALLATIONS	D-CBB-12A	06-28-19	TYPE 'B' CAST IRON FRAME, GRATE & VERTICAL INLET DETAILS FOR NOS. 10, 12, 14, 16 & 17 TYPE CATCH BASINS
PRESENT LAYOUT(S) .....	4 - 6	RD-L-3	02-20-20	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING	D-CBB-12D		TYPE "B" CAST IRON FRAME, GRATE & CURB HOOD DETAILS FOR NOS. 10, 12, 14, 16 & 17 TYPE CATCH BASINS
PROPOSED LAYOUT(S) .....	4A - 6A	RD-L-4	02-20-20	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING	D-MH-2	02-20-2020	STANDARD PRECAST NO. 3 MANHOLE
PROPOSED PROFILE(S) .....	4B - 5B	RD-L-5	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL	ROADWAY, PAVEMENT APPURTENANCES, AND FENCES		
SIDE ROAD PROFILES .....	7 - 8	RD-L-6	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL	RP-D-15	06-15-21	DETAILS OF STANDARD CONCRETE DRIVEWAYS
DRIVEWAY PROFILES .....	9 - 10	RD-L-7	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL	RP-SC-1	05-04-22	SLOPING CONCRETE CURB AND CURB AND GUTTER
EROSION PREVENTION AND SEDIMENT CONTROL PLANS.....	11, 12 - 13B	ROADWAY DESIGN STANDARDS			RP-VC-10	03-04-21	VERTICAL CONCRETE CURB AND CURB AND GUTTER (FOR 8" TO 12" GUTTER DEPTH)
SIGNING AND PAVEMENT MARKING PLANS .....	14 - 16	RD11-SE-1		TRANSITION AND CROSS SLOPE DETAILS	MULTIMODAL		
ROADWAY CROSS SECTIONS .....	17 - 16	RD11-SE-2		SUPERELEVATION TRANSITION DETAILS FOR UNDIVIDED ROADWAYS	MM-CR-1	06-28-19	DETECTABLE WARNING SURFACE PLACEMENT ON CURB RAMPS
TRAFFIC CONTROL PLANS .....	T1 – T2	RD11-SE-2A		SUPERELEVATION TRANSITION SECTIONS FOR UNDIVIDED ROADWAYS	MM-CR-5	06-28-19	SINGLE CROSSING CURB RAMP IN CURVE
LANDSCAPE PLAN .....	L1-00	RD11-TS-2		DESIGN STANDARDS FOR COLLECTORS, 2-LANE ROADS AND STREETS	MM-PM-1		SIGNING AND PAVEMENT MARKINGS AT INTERSECTION CROSSINGS FOR SHARED-USE PATHS
LANDSCAPE ENLARGEMENT .....	L1-10 – L1-11	RD11-TS-6		TYPICAL CURB & GUTTER SECTIONS WITH SHOULDERS AND WITH GRASS STRIPS	MM-SW-1	06-28-19	DETAILS FOR CONCRETE SIDEWALKS
LANDSCAPE NOTES & DETAILS .....	L1-50	RD11-TS-6A		TYPICAL CURB & GUTTER SECTIONS WITHOUT SHOULDERS AND WITH GRASS STRIPS	MM-SW-2		ALTERNATE DETAILS FOR CONCRETE SIDEWALK (REHABILITATION)
NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN THE NUMBERING OF SHEETS.		RD11-S-11		DESIGN AND CONSTRUCTION DETAILS FOR ROADSIDE SLOPE DEVELOPMENT	SAFETY DESIGN AND GUARDRAILS		
		RD11-S-11A		ROADSIDE DITCH DETAILS FOR DESIGN AND CONSTRUCTION	S-CZ-1	06-28-19	CLEAR ZONE CRITERIA
		RD11-SD-1		INTERSECTION SIGHT DISTANCE DESIGN AND GENERAL NOTES	DESIGN - TRAFFIC CONTROL		
		RD11-SD-2		INTERSECTION SIGHT DISTANCE LANDSCAPE AND OBSTRUCTION	T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
		RD11-SD-3		INTERSECTION SIGHT DISTANCE 2-LANE ROADWAYS	T-M-2	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
		RD01-S-11	04-04-03	DESIGN AND CONSTRUCTION DETAILS FOR ROADSIDE SLOPE DEVELOPMENT	T-M-4	07-17-20	STANDARD INTERSECTION PAVEMENT MARKINGS
		RD01-S-11A	10-15-02	ROADSIDE DITCH DETAILS FOR DESIGN AND CONSTRUCTION	T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
		PIPE CULVERTS AND ENDWALLS			T-WZ-55	10-29-21	SIDEWALK TRAFFIC CONTROL
		D-PB-1	03-04-21	STANDARD DETAILS FOR CONCRETE PIPE INSTALLATION	EROSION PREVENTION AND SEDIMENT CONTROL		
		D-PB-3	11-30-20	INDUCED TRENCH SOIL EMBANKMENT FOR PIPE CULVERT INSTALLATION	EC-STR-3B	06-15-21	SILT FENCE
		D-PE-15A	06-28-19	TYPE "U" CROSS DRAIN ENDWALL FOR 15" PIPE (FOR 3:1, 4:1 & 6:1 SLOPES)	EC-STR-6	11-30-20	ROCK CHECK DAM
		D-PE-18B	06-28-19	TYPE "U" CROSS DRAIN ENDWALL FOR 18" PIPE, BILL OF STEEL AND PRECAST NOTES	EC-STR-6A	05-06-16	ENHANCED ROCK CHECK DAM
					EC-STR-11	03-16-17	CULVERT PROTECTION TYPE 1
					EC-STR-19	04-01-08	CATCH BASIN PROTECTION
					EC-STR-39A	08-01-12	CURB INLET PROTECTION TYPE 3 & 4
					EC-STR-46		CATCH BASIN FILTER ASSEMBLY (TYPE 6)
					EC-STR-46A		CATCH BASIN FILTER ASSEMBLY (TYPE 6) SLIPCOVER DETAILS


Kimley»Ho

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COLLEGE STREET ROADWAY IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN



CITY of MARYVILLE  
*People are the KEY*



BY

DATE

REVISIONS

NO

1

2

3

4

5

6

7

8

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10

DESIGNED BY:

DRAWN BY:

CHECKED BY:

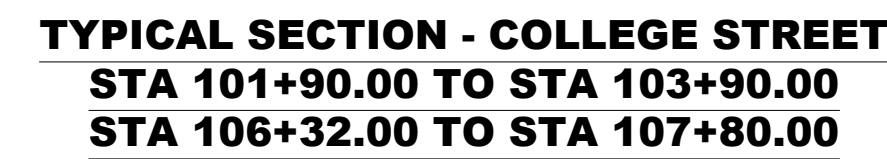
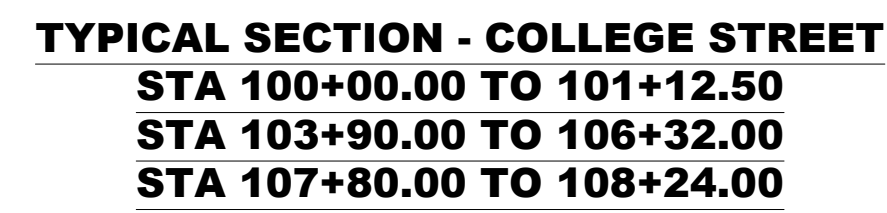
DATE:

KIMLEY-HORN PROJECT NO.  
118019006

ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS

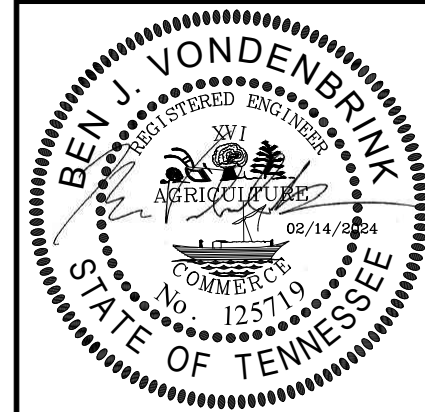
SHEET NUMBER  
1A

-CROSSWALK AREA TO BE REMOVED AT ALL  
INTERSECTIONS, TO BE REPLACED WITH FULL DEPTH  
-SEE DETAIL #6, SHEET 2G1



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[illegible]

No.	1	2	3	4	5	6	7	8	9	10
DESIGNED BY:	BJV									
DRAWN BY:	EJBF									
CHECKED BY:	DED									
DATE:	02/14/2024									
KIMLEY-HORN PROJECT NO. 118019006										

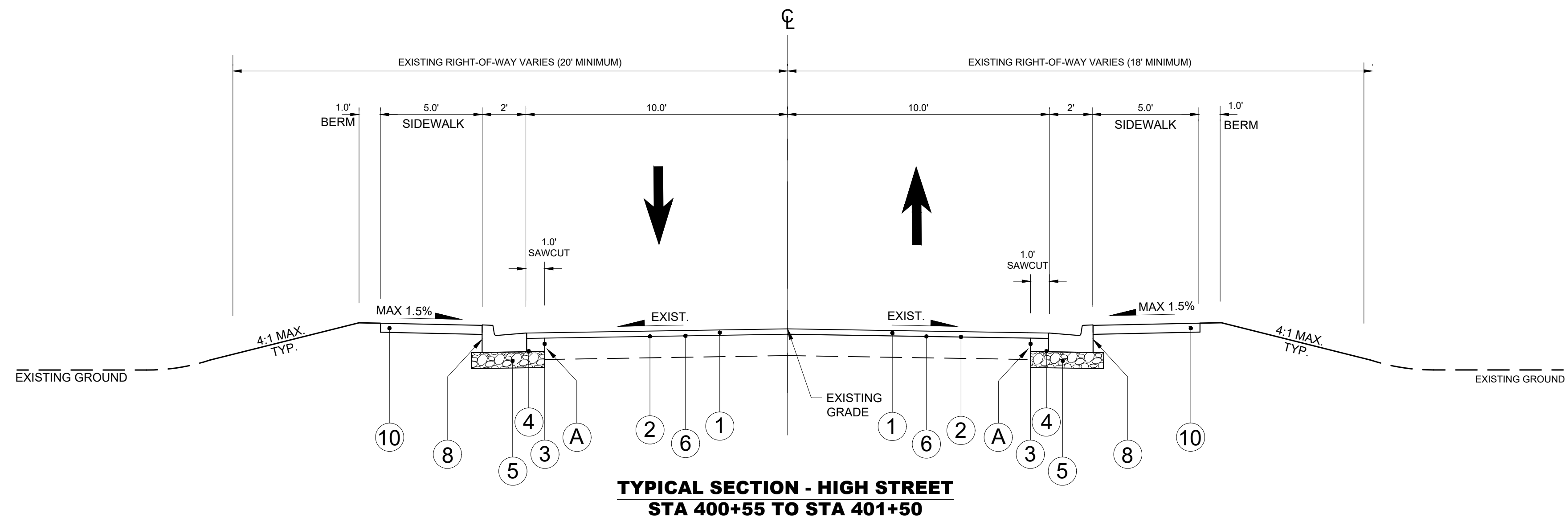
SHEET NUMBER

**2B**

EXISTING GROUND

4:1 MAX.  
TYP.

PROPOSED PAVEMENT SCHEDULE - ALL ROADWAYS	
①	ASPHALTIC CONCRETE SURFACES AT 1.5" THICK ITEM NO. 411-01.10 ACS MIX (PG64-22) GRADING D
②	TACK COAT ITEM NO. 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
③	CONCRETE AT 6.5" THICK ITEM NO. 501-01.01 PORTLAND CEMENT CONCRETE PAVEMENT (PLAIN)
④	PRIME COAT ITEM NO. 402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC) (0.30-0.35 GAL/SY) ITEM NO. 402-02 AGGREGATE FOR COVER MATERIAL (PC) (8-12 LB/SY)
⑤	MINERAL AGGREGATE BASE @ 4" THICK ITEM NO. 303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D
⑥	1.5" ASPHALT MILLING ITEM NO. 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
⑦	CONCRETE CURB ITEM NO. 702-01 CONCRETE CURB
⑧	6-24 CURB AND GUTTER ITEM NO. 702-03 CONCRETE COMBINED CURB AND GUTTER
⑨	6-30 CURB AND GUTTER ITEM NO. 702-03 CONCRETE COMBINED CURB AND GUTTER



SHEET NUMBER

**2B1**



**CONTRACTOR RESPONSIBILITIES:**

- PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL BE RESPONSIBLE FOR:**
- A. THE CONTRACTOR SHALL VERIFY ALL PROPOSED AND EXISTING CONDITIONS INCLUDING UTILITIES (INVERTS, CONNECTIONS, MATERIALS, ETC.) AND DIMENSIONS WITHIN THE LIMITS OF WORK PRIOR TO THE START OF CONSTRUCTION.
- B. REFER TO ARCHITECTURAL DRAWINGS FOR DETAILED BUILDING INFORMATION.
- C. THE CONTRACTOR IS RESPONSIBLE FOR ALL NOTIFICATIONS AND LIAISONS WITH UTILITY COMPANIES DURING THE PROCESS OF LOCATING, RELOCATING, AND TYING INTO PUBLIC UTILITIES.
- D. PRIOR TO COMMENCING LAND DISTURBANCE ACTIVITY, THE LIMITS OF LAND DISTURBANCE SHALL BE CLEARLY AND ACCURATELY DEMARCATED WITH STAKES, RIBBONS, OR OTHER APPROPRIATE MEANS. THE LOCATION AND EXTENT OF ALL AUTHORIZED LAND DISTURBANCE SHALL OCCUR INSIDE THE APPROVED LIMITS INDICATED ON THE APPROVED PLANS.
- 2. DURING CONSTRUCTION:**
- A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVIATIONS FROM THESE PLANS AND SPECIFICATIONS WITHOUT PRIOR WRITTEN CONSENT OF THE ENGINEER MAY CAUSE THE WORK TO BE UNACCEPTABLE.
- B. THE CONTRACTOR SHALL USE MATERIALS AND EMPLOY CONSTRUCTION METHODS IN ORDER TO COMPLY WITH THE DRAWINGS AND SPECIFICATIONS. WHERE A CONFLICT OCCURS, THE STRICTEST DESIGN SHALL GOVERN. THE ENGINEER'S REVIEW OF SHOP DRAWINGS, PRODUCT DATA, ETC., DOES NOT RELIEVE THE CONTRACTOR FROM COMPLYING WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL INFORM THE ENGINEER IN WRITING OF ANY SPECIFIC DEVIATIONS AND OBTAIN ENGINEER'S WRITTEN APPROVAL OF THE SPECIFIC DEVIATION.
- C. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL APPLICABLE SAFETY CODES AND REGULATIONS DURING ALL PHASES OF CONSTRUCTION.
- D. ALL CONSTRUCTION MUST CONFORM TO THE STANDARDS, SPECIFICATIONS, AND CODES OF THE GOVERNING MUNICIPALITIES.
- E. CONSTRUCTION SHALL MEET ALL CURRENT STANDARDS SET FORTH IN THE AMERICANS WITH DISABILITIES ACT.
- F. IF THE CONTRACTOR DAMAGES ANY EXISTING UTILITIES DURING CONSTRUCTION, HE SHALL, AT HIS OWN EXPENSE, REPLACE OR REPAIR THE UTILITIES TO ORIGINAL CONDITION AND QUALITY AS APPROVED BY THE OWNER AND REPRESENTATIVE OF THE APPROPRIATE UTILITY COMPANY.
- G. SUFFICIENT BARRICADES, LIGHTS, SIGNS, AND OTHER TRAFFIC CONTROL METHODS IN ACCORDANCE WITH GOVERNING ORDINANCES MAY BE NECESSARY FOR THE PROTECTION AND SAFETY OF THE PUBLIC. SAID CONTROL DEVICES SHALL BE PER THE MANUAL OF TRAFFIC CONTROL DEVICES, M.U.T.C.D., CURRENT EDITION, AND SHALL BE PROVIDED AND MAINTAINED THROUGHOUT CONSTRUCTION.
- H. TRAFFIC CONTROLS AND OTHER WARNING DEVICES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF ANY WORK ON CITY, COUNTY, OR TENNESSEE DEPARTMENT OF TRANSPORTATION ROADS. THEY SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION AND SHALL REMAIN IN PLACE UNTIL THE CONCLUSION OF ALL WORK.
- I. ALL WARNING DEVICES SHALL BE EITHER TYPE I BARRICADES OR DRUMS WITH WARNING LIGHTS ON EVERY OTHER DEVICE. THEY SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITION, AND CITY OF MARYVILLE STANDARDS FOR COLOR, SIZE, REFLECTIVITY, HEIGHT, AND PLACEMENT.
- J. FIRE DEPARTMENT ACCESS SHALL BE MAINTAINED AT ALL TIMES.
- K. CONTRACTOR SHALL SHORE AND BRACE ALL EARTH, FORMS, CONCRETE, STEEL, WOOD, AND MASONRY TO RESIST GRAVITY, EARTH, WIND, THERMAL, CONSTRUCTION, AND MISCELLANEOUS LOADS DURING CONSTRUCTION.
- L. ON-SITE BURIAL OF DEBRIS IS PROHIBITED.
- M. UNLESS OTHERWISE NOTED THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF ALL FABRICATED MATERIALS TO THE ENGINEER. DESIGN DOCUMENTS SHALL NOT BE REPRODUCED AS SHOP DRAWINGS.
- N. IN CASE OF UNFORESEEN CONSTRUCTION COMPLICATIONS OR DISCREPANCIES, THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE ENGINEER IN WRITING.
- O. ALL REQUIRED TESTING REPORTS SHALL BE AVAILABLE AT THE JOB SITE.
- P. AS-BUILT DRAWINGS OF ROADWAYS, STORM DRAINS, SANITARY SEWER AND WATER LINES, FIELD APPROVAL BY THE ENGINEER, AND ALL APPLICABLE BONDS ARE REQUIRED PRIOR TO FINAL ACCEPTANCE BY THE OWNER.
- Q. CONTRACTOR SHALL MAINTAIN CONTINUOUS UTILITY SERVICE TO ALL EXISTING BUILDINGS THROUGHOUT CONSTRUCTION UNLESS APPROVAL FOR SERVICE INTERRUPTION IS OBTAINED FROM THE OWNERS IN ADVANCE.
- R. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS TO ENSURE THAT THE NEW WORK SHALL FIT INTO THE EXISTING SITE IN THE MANNER INTENDED AND AS SHOWN ON THE DRAWINGS. SHOULD ANY CONDITIONS EXIST THAT ARE CONTRARY TO THOSE SHOWN ON THE DRAWINGS, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE PRIOR TO PERFORMING ANY WORK IN THE AREA INVOLVING DIFFERENCES. NOTIFICATION SHALL BE IN THE FORM OF A DRAWING OR SKETCH INDICATING FIELD MEASUREMENTS AND NOTES RELATING TO THE AREA.
- S. ANY FOREIGN ITEM FOUND DURING CONSTRUCTION IS THE PROPERTY OF THE OWNER. THIS INCLUDES, BUT IS NOT LIMITED TO, PRECIOUS METALS, COINS, PAPER CURRENCY, ARTIFACTS AND ANTIQUITIES.
- T. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE DONE TO THE PREMISES OR ADJACENT PREMISES, OR INJURIES TO THE PUBLIC DURING THE CONSTRUCTION OF THE WORK, WHETHER CAUSED BY HIMSELF, HIS SUBCONTRACTORS, OR THE CARELESSNESS OF ANY OF HIS EMPLOYEES.
- U. THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN ALL NECESSARY TEMPORARY WORKS FOR THE PROTECTION OF THE WORK AND THE PUBLIC, INCLUDING BARRICADES, WARNING SIGNS, LIGHTS, ETC.
- V. THE CONTRACTOR ACKNOWLEDGES & AGREES THAT THE WORK IS ENTIRELY AT HIS RISK UNTIL SITE IS ACCEPTED AND HE WILL BE HELD RESPONSIBLE FOR ITS SAFETY BY THE OWNER. THE CONTRACTOR WILL INDEMNIFY THE OWNER & OWNER'S REPRESENTATIVE FROM LIABILITY AT THE SITE THROUGHOUT THE CONSTRUCTION PROCESS.
- W. THE CONTRACTOR SHALL GIVE ALL NECESSARY NOTICES AND OBTAIN ALL PERMITS AND PAY ALL LEGAL FEES. HE SHALL ALSO COMPLY WITH ALL CITY, COUNTY AND STATE BUILDING LAWS, ORDINANCES OR REGULATIONS RELATING TO BUILDING SIDEWALKS, STREETS, BLASTING, PUBLIC INFRASTRUCTURE, STORMWATER REGULATIONS, ETC.
- X. THE CONTRACTOR IS TO CHECK AND VERIFY ALL MEASUREMENTS, LEVELS, ETC. BEFORE ORDERING MATERIALS AND PROCEEDING WITH THE WORK, AND IS TO BE RESPONSIBLE FOR THE SAME.
- Y. REFERENCE POINTS AND HUBS DURING THE CONSTRUCTION OF HIS WORK, AND SHALL BEAR THE COST OF REPLACING SAME.
- Z. CARE SHALL BE TAKEN TO PROTECT ANY UTILITIES, TREES, ETC. WHICH ARE TO REMAIN AND NOT TO BE DISTURBED BY THE CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGES TO SUCH PROPERTY.
- AA. LANE CLOSURES MUST BE APPROVED BY THE CITY ENGINEER MUST SUBMIT TRAFFIC CONTROL PLAN, WORK SCHEDULE AND LANE CLOSURE REQUEST BY FRIDAY THE WEEK BEFORE THE WORK START DATE.

**DEMOLITION INFORMATION:**

- NOTIFICATIONS:
- THE CONTRACTOR SHALL NOTIFY THE OWNER AND CITY INSPECTOR(S) 24 HOURS PRIOR TO ANY DEMOLITION OR CONSTRUCTION.
2. DISPOSAL GUIDELINES:
- A. ONLY ITEMS SPECIFICALLY NOTED TO BE DEMOLISHED SHALL BE REMOVED FROM THE SITE.
- B. REMOVE EXISTING PAVED AREAS AS SHOWN INCLUDING DRIVEWAYS, SIDEWALKS, PARKING AREAS, SERVICE AREAS, EQUIPMENT PADS, AND ALL MISCELLANEOUS PAVING.
- C. ALL DEBRIS RESULTING FROM DEMOLITION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF LEGALLY BY THE CONTRACTOR IN ACCORDANCE WITH LOCAL STATE AND FEDERAL REGULATIONS. BACKFILL ALL TRENCHES AND EXCAVATIONS RESULTING FROM DEMOLITION.
- D. ALL DEMOLISHED MATERIAL BECOMES THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE NOTED.
3. TREE PROTECTION GUIDELINES:
- PROTECT ALL EXISTING TREES NOTED "TO REMAIN" AND ALL ITEMS TO BE TURNED OVER TO THE OWNER DURING DEMOLITION. TAKE ALL NECESSARY PRECAUTIONS AND PROTECTIVE MEASURES. ANY EXISTING ITEMS TO BE TURNED OVER TO THE OWNER WHICH ARE DAMAGED DURING DEMOLITION SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER. TREES WHICH ARE DAMAGED WILL BE REPLACED OR REIMBURSED AT A RATE TO BE DETERMINED BY THE OWNER.
4. UTILITIES:
- A. PRIOR TO REMOVING OR ABANDONING ANY UTILITY THE CONTRACTOR SHALL VERIFY THAT NO UPSTREAM SERVICE WILL BE TERMINATED. THE CONTRACTOR SHALL INFORM THE ENGINEER IN WRITING OF ANY TERMINATION NOT SHOWN ON THE PLANS.
- B. ALL ABANDONED WATER LINES, STORM SEWER PIPE, SANITARY SEWER PIPES, GAS LINES, OR ANY OTHER ABANDONED UNDERGROUND UTILITY SHALL BE ABANDONED IN PLACE UNLESS NOTED OTHERWISE.

### SITE INFORMATION:

1. THE FOLLOWING ARE APPLICABLE TO ALL CIVIL DOCUMENTS:
- A. WHERE A DETAIL SECTION, TYPICAL SECTION, OR A NOTE IS SHOWN FOR ONE CONDITION, IT SHALL APPLY FOR ALL LIKE OR SIMILAR CONDITIONS, UNLESS OTHERWISE NOTED ON THE PLANS.
  - B. EXISTING AND PROPOSED CONTOURS ARE AT ONE (1) FOOT INTERVALS.
  - C. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - D. ALL PIPE LENGTHS SPECIFIED IN THESE PLANS ARE THE HORIZONTAL DISTANCE AND ARE SHOWN FOR REFERENCE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE ACTUAL LENGTHS BASED ON PROPOSED PIPE SLOPE.
  - E. PIPE LENGTHS IN PLANS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE UNLESS OTHERWISE NOTED.

**EROSION AND SEDIMENT CONTROL INFORMATION:**

- COMPREHENSIVE:
- A. THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO OR CONCURRENT WITH LAND-DISTURBING ACTIVITIES.
- B. PROVISIONS TO PREVENT EROSION OF SOIL FROM THE SITE SHALL BE AT A MINIMUM IN CONFORMANCE WITH THE REQUIREMENTS OF THE TENNESSEE EROSION AND SEDIMENT CONTROL HANDBOOKS. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- C. FAILURE TO INSTALL, OPERATE, OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON THE JOB SITE UNTIL SUCH MEASURES ARE CORRECTED BACK TO THE STANDARDS SPECIFIED IN THE TENNESSEE EROSION AND SEDIMENT CONTROL HANDBOOKS, CURRENT EDITION.
- D. EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- E. EROSION CONTROL DEVICES SHALL BE INSTALLED PRIOR TO LAND DISTURBANCE. THE LOCATION OF SOME OF THE EROSION CONTROL DEVICES MAY HAVE TO BE ALTERED FROM THAT SHOWN ON THE APPROVED PLANS IF DRAINAGE PATTERNS DURING CONSTRUCTION ARE DIFFERENT FROM THE FINAL PROPOSED DRAINAGE PATTERNS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ACCOMPLISH EROSION CONTROL FOR ALL DRAINAGE PATTERNS CREATED AT VARIOUS STAGES DURING CONSTRUCTION. ANY DIFFICULTY IN CONTROLLING EROSION DURING ANY PHASE OF CONSTRUCTION SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.
- F. THE CONSTRUCTION OF THE SITE WILL COMMENCE WITH THE INSTALLATION OF EROSION CONTROL MEASURES SUFFICIENT TO CONTROL SEDIMENT DEPOSITS AND EROSION. ALL SEDIMENT CONTROL WILL BE MAINTAINED UNTIL ALL UPSTREAM GROUND WITHIN THE CONSTRUCTION AREA HAS BEEN COMPLETELY STABILIZED WITH PERMANENT VEGETATION AND ALL ROADS/DRIVEWAYS HAVE BEEN PAVED.
- G. CONSTRUCTION EXITS SHALL BE CONSTRUCTED AT EACH POINT OF ENTRY OR EXIT FROM THE SITE AND SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH STONE AS CONDITIONS DEMAND, REPAIR AND/OR CLEANOUT OF ANY STRUCTURES USED TO TRAP SEDIMENT, ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES OFF SITE ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY. ACCESS POINTS PROTECTED WITH A CONSTRUCTION EXIT SHALL BE OTHERWISE BARRICADED UNTIL THE SITE IS STABILIZED.

### ADA COMPLIANCE:

- A. CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED BASED ON THE CITY STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.
- B. PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
- C. ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.
- D. ANY COMPONENTS OF THE PROJECT SERVING MULTIFAMILY DWELLINGS IN BUILDINGS THAT HAVE 4 OR MORE UNITS PER DWELLING SHALL ALSO CONFORM TO THE FAIR HOUSING ACT (FHA), AND COMPLY WITH THE FAIR HOUSING ACT DESIGN MANUAL BY THE US DEPARTMENT OF HOUSING AND URBAN DEVELOPMENT.

### CLEARING AND GRUBBING:

- A. DO NOT EXCEED CLEARING AND GRUBBING LIMITS OF CONSTRUCTION LINES INDICATED ON THE PLANS.
- B. ALL AREAS OUTSIDE THE LIMITS OF CONSTRUCTION LINE SHALL NOT BE CROSSED BY HEAVY EQUIPMENT OR USED FOR STORING HEAVY EQUIPMENT OR MATERIALS.
- C. NO EQUIPMENT SHALL BE STORED UNDER THE DRIP LINE OF TREES TO REMAIN.
- D. DO NOT FALL ANY TREES OR PUSH PILES OF DEBRIS AGAINST ANY TREES TO REMAIN.
- E. REMOVE ALL STUMPS, ROCKS, ASPHALT & CONCRETE DEBRIS, ETC. AND DISPOSE OFF SITE IN ACCORDANCE WITH LOCAL, STATE & FEDERAL REGULATIONS.
- F. CONTACT ALL UTILITY AUTHORITIES WHO HAVE LINES WITHIN THE CLEARING AND GRUBBING LIMITS BEFORE STARTING WORK
- E. ALL EROSION CONTROL SEDIMENT BARRIERS, SILT FENCES, AND TREE PROTECTION DEVICES SHALL BE INSTALLED PRIOR TO STARTING CLEARING AND GRUBBING.
- F. AFTER STAKING IS COMPLETED, TREES WITHIN GRADING LIMITS TO BE SAVED WILL BE IDENTIFIED BY THE OWNER'S REPRESENTATIVE. FIELD CHANGES TO GRADING PLANS SHALL BE MADE FOR SMOOTH TRANSITION OF GRADES AROUND ALL TREES WHICH REQUIRE TREE WELLS WITHIN THE GRADING LIMITS.
- G. ALL CLEARING SHALL BE LIMITED TO AREAS TO BE GRADED WITHIN 15 CALENDAR DAYS

**PAVEMENT INFORMATION:**

1. PAVEMENT:
  - A. ALL MATERIALS, EQUIPMENT, METHODS OF CONSTRUCTION, AND WORKMANSHIP SHALL CONFORM TO THE TENNESSEE DEPARTMENT OF TRANSPORTATION, TDOT, STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION.
  - B. SEE PAVEMENT DETAILS ON CONSTRUCTION DOCUMENTS FOR SPECIFIC DESIGN INFORMATION AND REQUIREMENTS.
  - C. ALL CURB AND GUTTER TO BE 24" AND CONSTRUCTED OF 3000 P.S.I. CONCRETE UNLESS OTHERWISE NOTED.
2. SIGNING AND STRIPING:
  - A. SIGNING AND STRIPING TO BE PROVIDED BY THE CONTRACTOR ACCORDING TO THE DRAWINGS AND SPECIFICATIONS.
  - B. ALL PAVEMENT MARKINGS SHALL CONFORM TO CURRENT MUTCD STANDARDS. ALL PAVEMENT MARKINGS ON PRIVATE PROPERTY SHALL BE PAINT, UNLESS NOTED OTHERWISE. ALL PAVEMENT MARKINGS ON PUBLIC RIGHT-OF-WAY SHALL BE THERMOPLASTIC, UNLESS NOTED OTHERWISE.

BY		DATE		REVISIONS	
No.	1	2	3	4	5
DESIGNED BY:				BJV	
DRAWN BY:				EJBF	
CHECKED BY:				DED	
DATE:				02/14/2024	
KIMLEY-HORN PROJECT NO. 118019006					
GENERAL NOTES					
SHEET NUMBER <b>2C</b>					

Drawing name: K:\NSH\_Roadway\118019006 - Maryville Streetscape\Cadd\Plans\002D - Tabulated Quantities.dwg 2D - TABULATED QUANTITIES Feb 14, 2024 1:26pm by: Ben.Vondenbrink

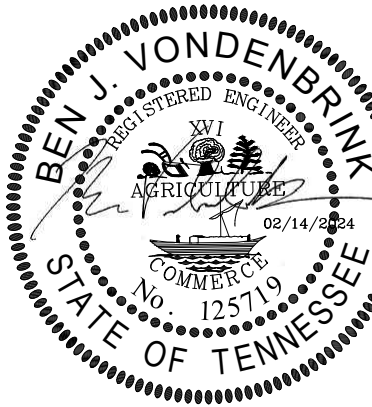
CATCH BASINS														
SHEET NO.	LOCATION	STATION	OFFSET (FT.)	DRAINAGE CODE	GRATE/TOP ELEV.	STRUCTURE TYPE	INSIDE DIMENSION	DEPTH (FT.)	STANDARD DRAWINGS	TYPE 12 C.B. 611-12.01 0' - 4'	TYPE 42 C.B. 611-42.01 0' - 4'	TYPE 42 C.B. 611-42.07 4' - 8'	TYPE 3 M.H. 611-01.02 4' - 8'	REMARKS
5A	ELLIS	300+55.00	12.0	CB-01	920.84	#42	3'X4'	3.94	D-CB-42RB		1			
5A	COLLEGE	105+25.00	10.0	CB-02	923.67	#42	3'X4'	4.72	D-CB-42RB			1		
5A	COLLEGE	104+90.00	-13.35	CB-03	920.17	#42	5'	4.05	D-CB-42RB			1		
5A	HIGH	401+30.00	-10	CB-04	943.68	#12	5'	3.94	D-CB-12RB	1				
5A	HIGH	401+35.00	10.5	CB-05	943.75	#12	5'	3.94	D-CB-12RB	1				
6A	MARYVILLE	104+90.00	-2.46	MH-01	920.09	MH #3	5'	6.00	D-MH-2				1	Replace existing catch basin with manhole.
6A	HIGH	401+15.00	1.75	MH-02	943.54	MH #3	5'	5.75	D-MH-2				EX	Existing manhole to remain in place.
TOTALS										2	1	2	1	

STORM DRAINAGE PIPES						
SHEET  NO.	FROM		TO		%	RCP CLASS III
	CODE	OUTLET ELEV.	CODE	INLET ELEV.		GRADE
5A	CB-01	916.90	MH-01	916.00	3.24	28
5A	CB-02	919.00	MH-01	916.00	9.32	32
5A	CB-03	915.98	EX-01	914.14	11.95	EX
5A	MH-01	915.49	EX-01	914.14	9.57	EX
6A	CB-04	939.74	EX-02	939.40	2.00	17
6A	CB-05	939.81	EX-02	939.40	2.11	19
TOTALS						97

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COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN



BY	DATE	REVISIONS	No.
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			2
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DESIGNED BY:	B
DRAWN BY:	EJ
CHECKED BY:	D
DATE:	02/14/20
KIMLEY-HORN PROJECT NO. 118019006	

TABULATED  
QUANTITIES

SHEET NUMBER

## 2D



EXISTING OFF-ROADWAY PAVEMENT

EXIST. ROW

SLOPE EASEMENT

5'

VAR.

1.25" SURFACE, ITEM NO. 411-01.10

6:1 (TYP.)

10:1 MAX. IN PARKING AREAS

4" BASE, ITEM NO. 303-01

2" BINDER, ITEM NO. 307-01.08

SIDEWALK

GRASS STRIP

Diagram illustrating a cross-section of a road construction project, showing a cut section on the left and a fill section on the right, separated by a central road section.

**Key Components and Labels:**

- CUT SECTION:** The left side of the diagram, showing the road cutting into the existing ground.
- FILL SECTION:** The right side of the diagram, showing the road being built up from the existing ground.
- EXISTING GROUND:** Indicated by dashed lines on both the left and right sides.
- 10' MIN. WIDTH:** The minimum width of the road section.
- VARIES:** A dimension indicating the width of the cut section.
- 3:1 MAX.:** The maximum slope ratio for the cut and fill sections.
- PGL:** A vertical line representing the centerline or a specific layer.
- SURFACE ITEM NO. 411-01.10, ACS MIX (PG64-22), GRADING D RDWY:** The top layer of the road.
- BASE, ITEM NO. 303-01, MINERAL AGGREGATE, TYPE A BASE, GRADING D:** The layer below the surface.

**Table:**

SURFACE	- 1.5"
BASE	- 8"

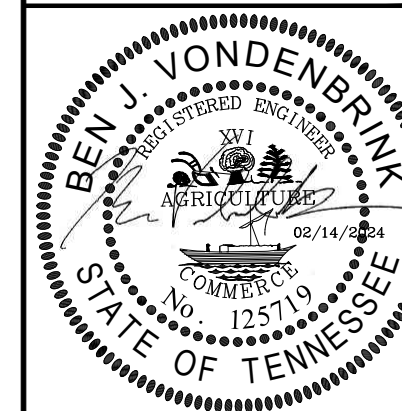
Technical drawing of a mechanical part with the following dimensions and tolerances:

- Overall width: 24" (6" + 18")
- Overall height: 13.5" (6.5" + 8")
- Top horizontal segment: 6"
- Top horizontal segment: 18"
- Top horizontal segment: 1"
- Top horizontal segment: 1/4"
- Top horizontal segment: 1/2" R
- Top horizontal segment: 1" R
- Top horizontal segment: 0.085
- Top horizontal segment: 9.5"
- Top horizontal segment: 2'-0"

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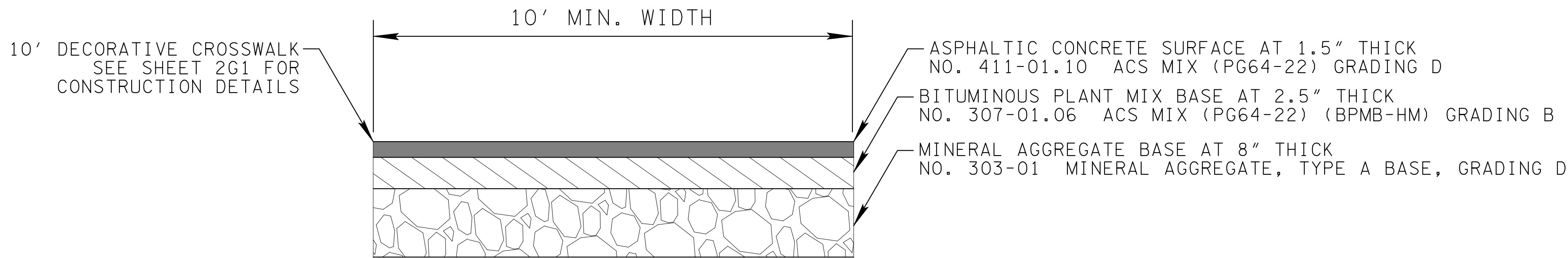
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CHECKED BY:									D
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DETAIL SHEET

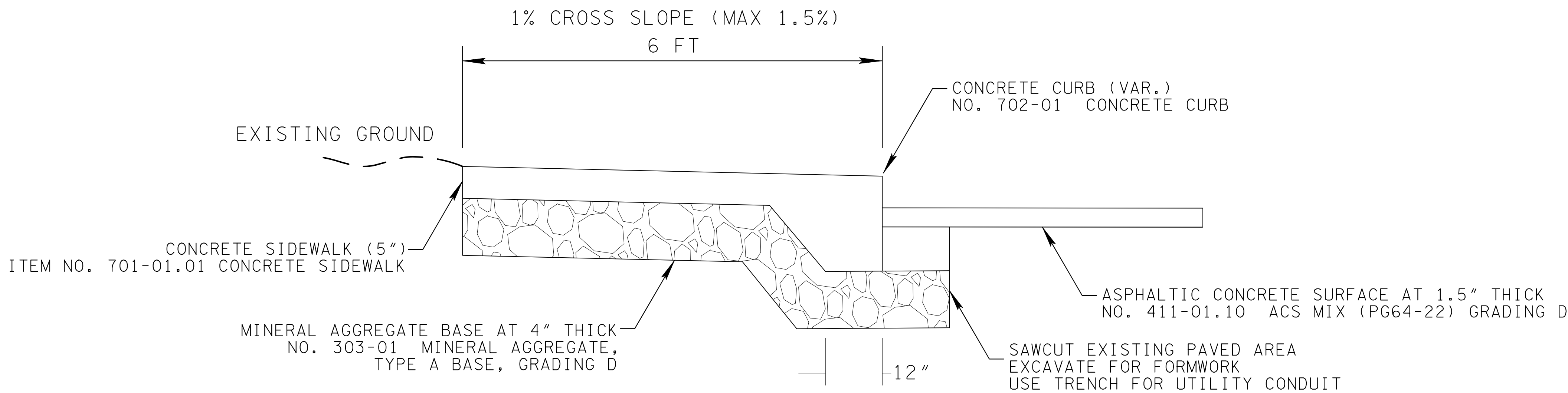
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## 6 CROSSWALK FULL DEPTH REPLACEMENT

SCALE: N.T.S.



\*IN DRIVEWAY APRON AREAS, INCREASE TO 6" THICKNESS & ADD 6X6 W.W.M.

## 7 TYPICAL 5" THICK INTEGRAL CURB SIDEWALK (4" CURB HEIGHT)

SCALE: N.T.S.

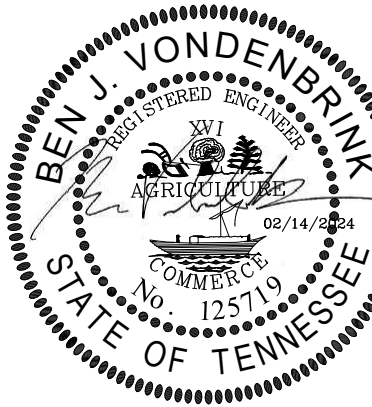
MIN 4,000 PSI CONCRETE W/AIR ENTRAINMENT

NOTE:  
CONTRACTOR SHALL ROUND EDGES TO 3/4"  
DO NOT USE "PICTURE WINDOW" EDGES  
USE LIGHT BROOM FINISH & CONCRETE SEALER

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DETAIL SHEET

SHEET NUMBER  
**2G1**








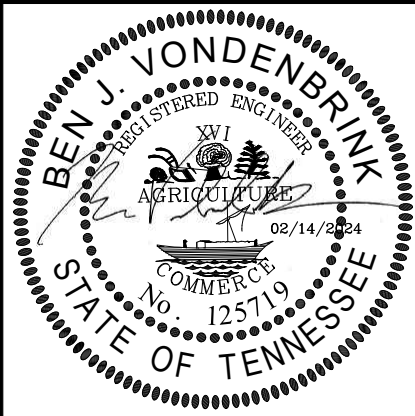
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GRAPHIC SCALE IN FEET

0 10 20 40



A graphic scale bar showing distances in feet, with markings at 0, 10, 20, and 40. To the right of the scale is a north arrow pointing towards the top right, with the word 'NORTH' written along its shaft.



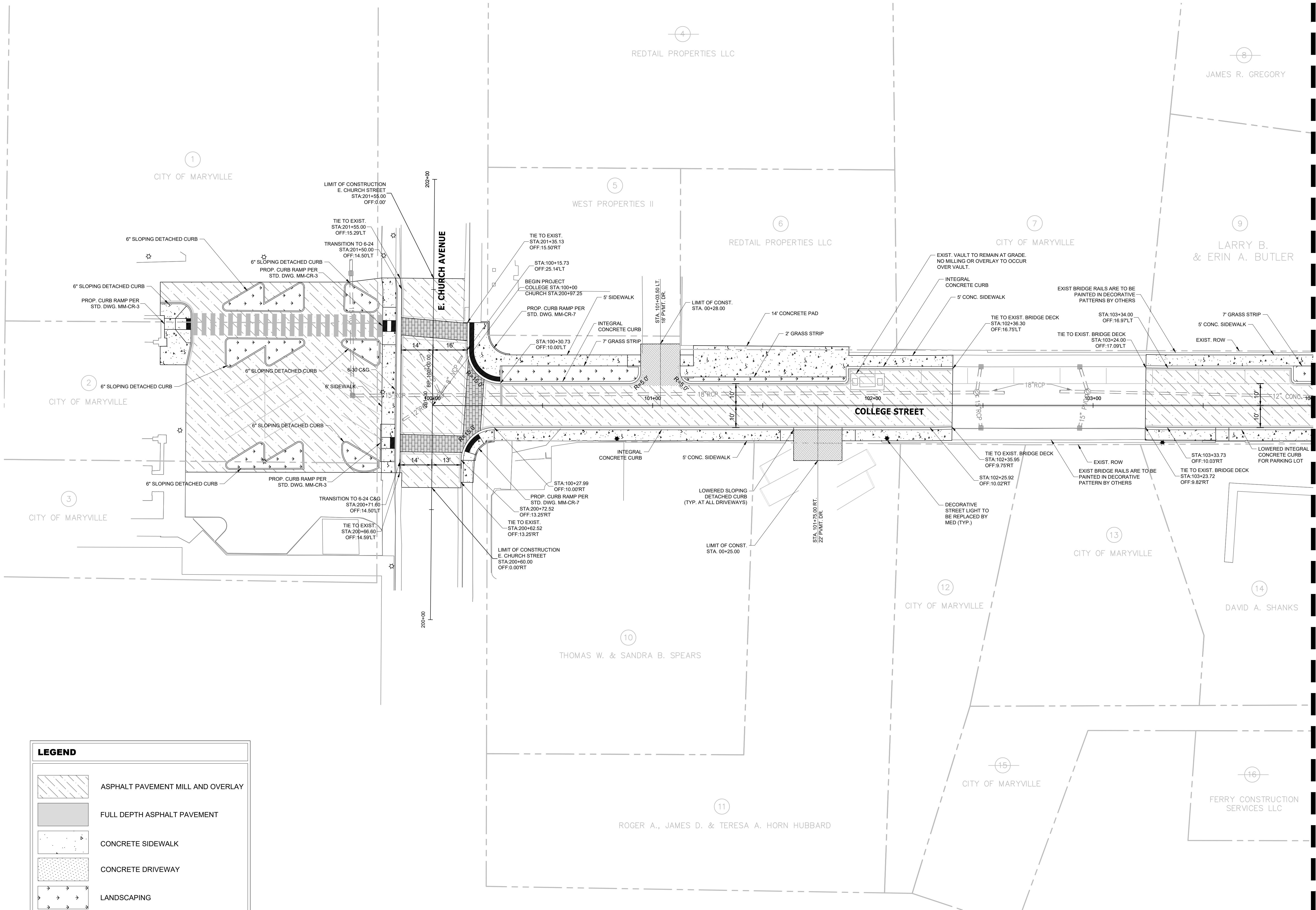
PRESENT LAYOUT

SHEET NUMBER

**4**

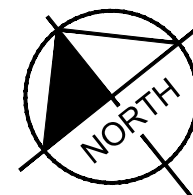
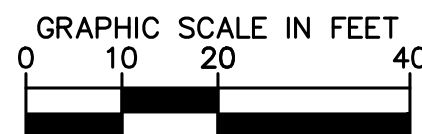


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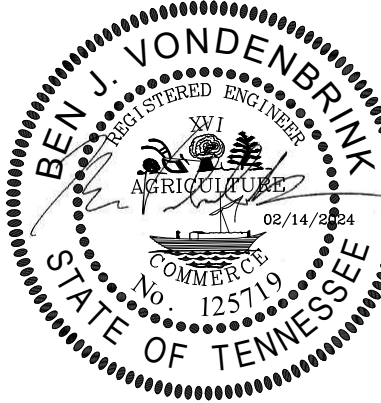


LEGEND

- ASPHALT PAVEMENT MILL AND OVERLAY
- FULL DEPTH ASPHALT PAVEMENT
- CONCRETE SIDEWALK
- CONCRETE DRIVEWAY
- LANDSCAPING
- BRICK



COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN



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KIMLEY-HORN PROJECT NO. 118019006			

PROPOSED LAYOUT

SHEET NUMBER

4A

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**BEGIN PROJECT**  
COLLEGE STREET STA: 100+00.00  
E. CHURCH AVE STA: 200+97.25  
ELEV: 925.69

**EXISTING GROUND**

**PROPOSED GRADE**

**LIMIT OF CONSTRUCTION**  
TIE TO EXIST. BRIDGE DECK  
COLLEGE STREET STA: 102+36.08  
ELEV: 914.26

**LIMIT OF CONSTRUCTION**  
TIE TO EXIST. BRIDGE DECK  
COLLEGE STREET STA: 103+23.82  
ELEV: 914.59

**EXISTING ELEVATION**

**PROPOSED ELEVATION**

**Vertical Curve Data:**

Curve	BVC	PVI	ELEV	AD	K	VC
1	101+39.73	101+64.73	916.38	2.67%	18.73	50.00'
2	101+69.73	101+94.73	915.33	2.86%	18.73	50.00'
3	102+36.08	102+61.08	914.26	2.86%	18.73	50.00'
4	103+23.82	103+48.82	914.59	1.10%	31.92	35.00'

**Grades:**

- 5.65%
- 2.86%
- 3.85%

**Stationing:** 100+00, 101+00, 102+00, 103+00, 104+00

**Elevations:** 900, 905, 910, 915, 920, 925, 930, 935

GRAPHIC SCALE IN FEET

0 10 20 40

HORIZONTAL

0 1 2 4

VERTICAL





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MATCHLINE STA. 104+00 SEE SHEET 4A

MATCHLINE SEE SHEET 6A

**LEGEND**

ASPHALT PAVEMENT MILL AND OVERLAY

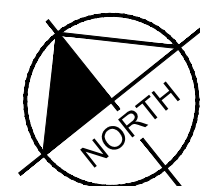
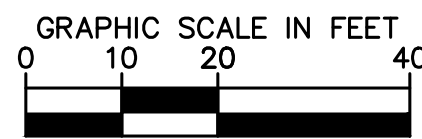
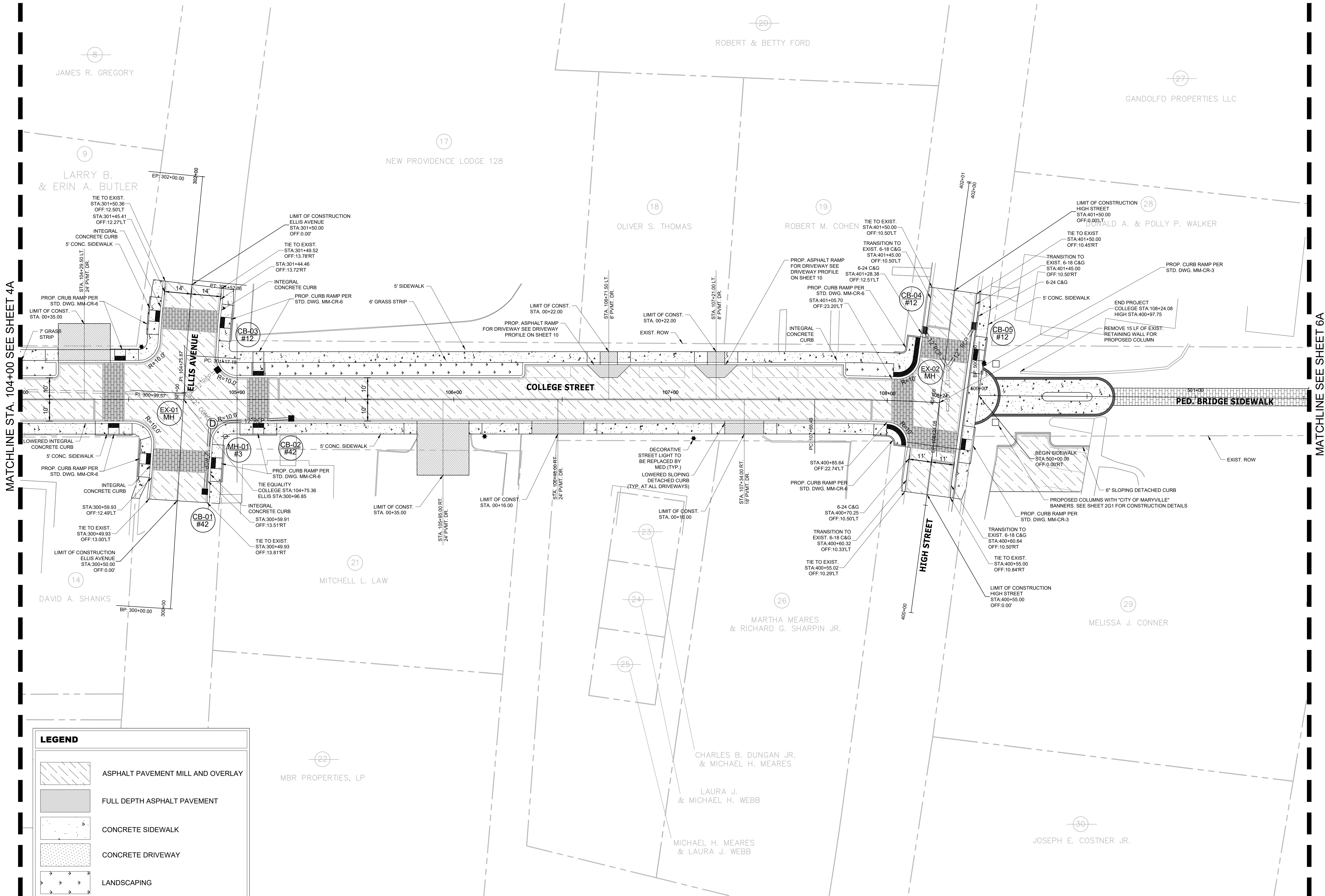
FULL DEPTH ASPHALT PAVEMENT

CONCRETE SIDEWALK

CONCRETE DRIVEWAY

LANDSCAPING

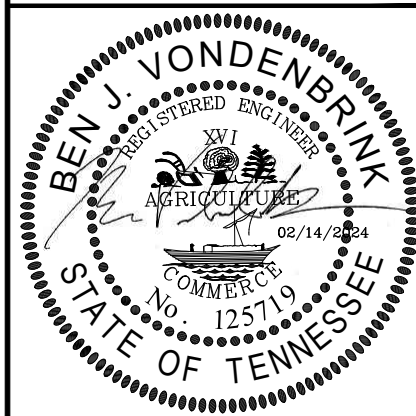
BRICK



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COLLEGE STREET ROADWAY  
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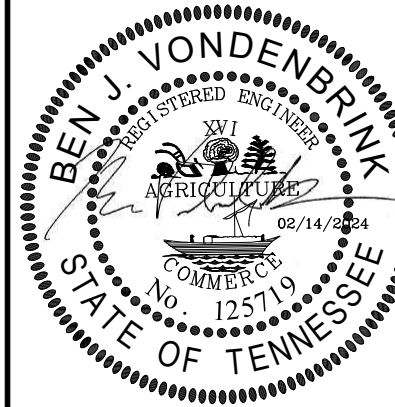


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DRAWN BY:		EJBF								
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PROPOSED LAYOUT

SHEET NUMBER

5A



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## MAINLINE PROFILE

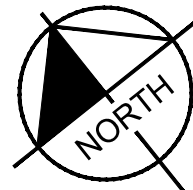
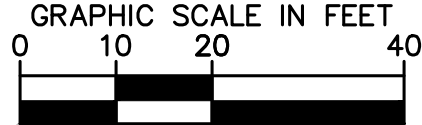
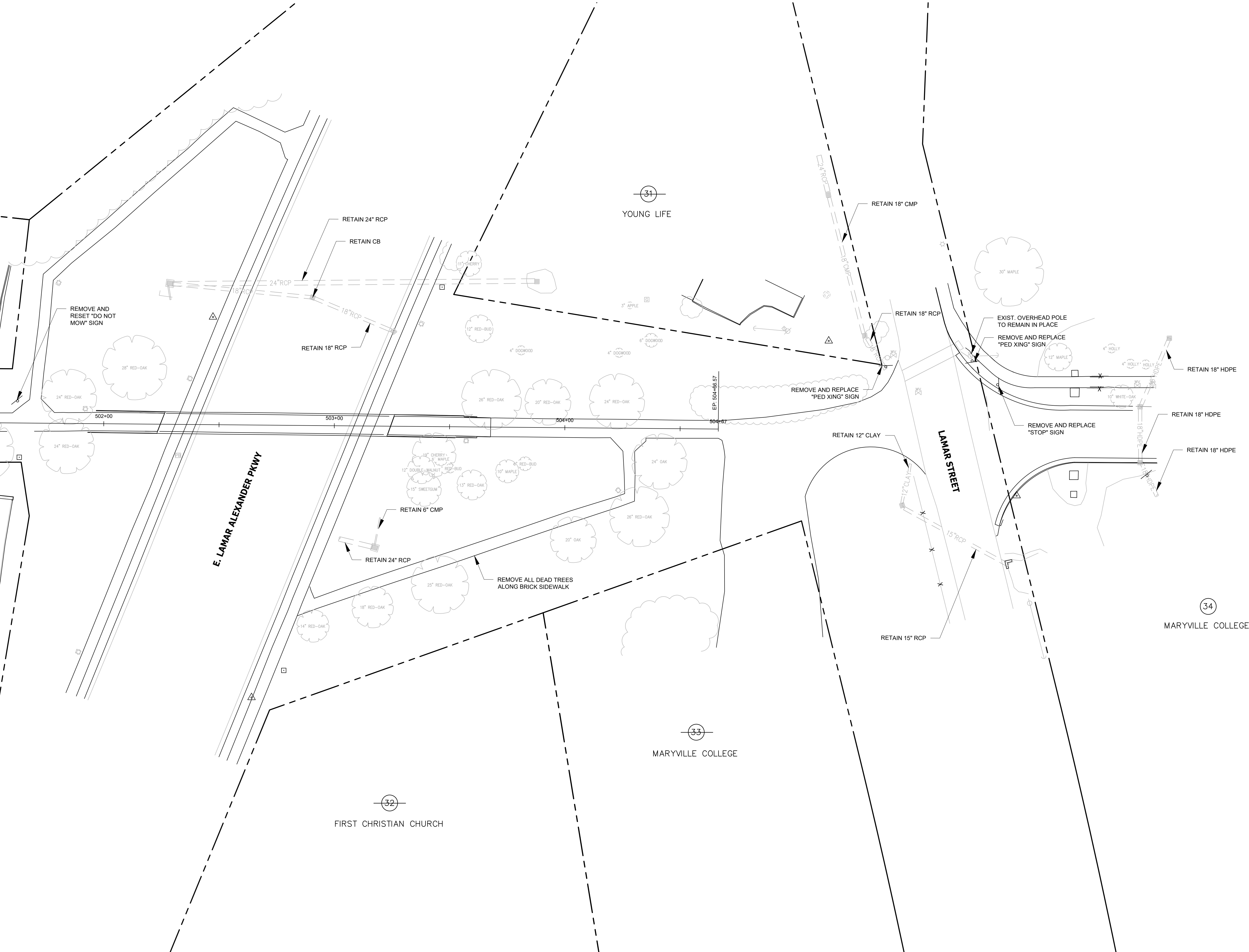
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MATCHLINE SEE SHEET 5



COLLEGE STREET ROADWAY  
IMPROVEMENTS  
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PRESENT LAYOUT

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6

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MATCHLINE SEE SHEET 5A

**LEGEND**

ASPHALT PAVEMENT MILL AND OVERLAY

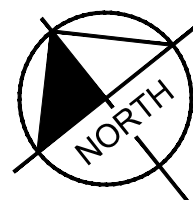
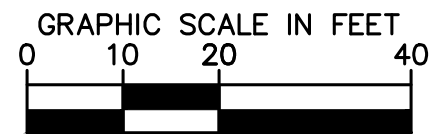
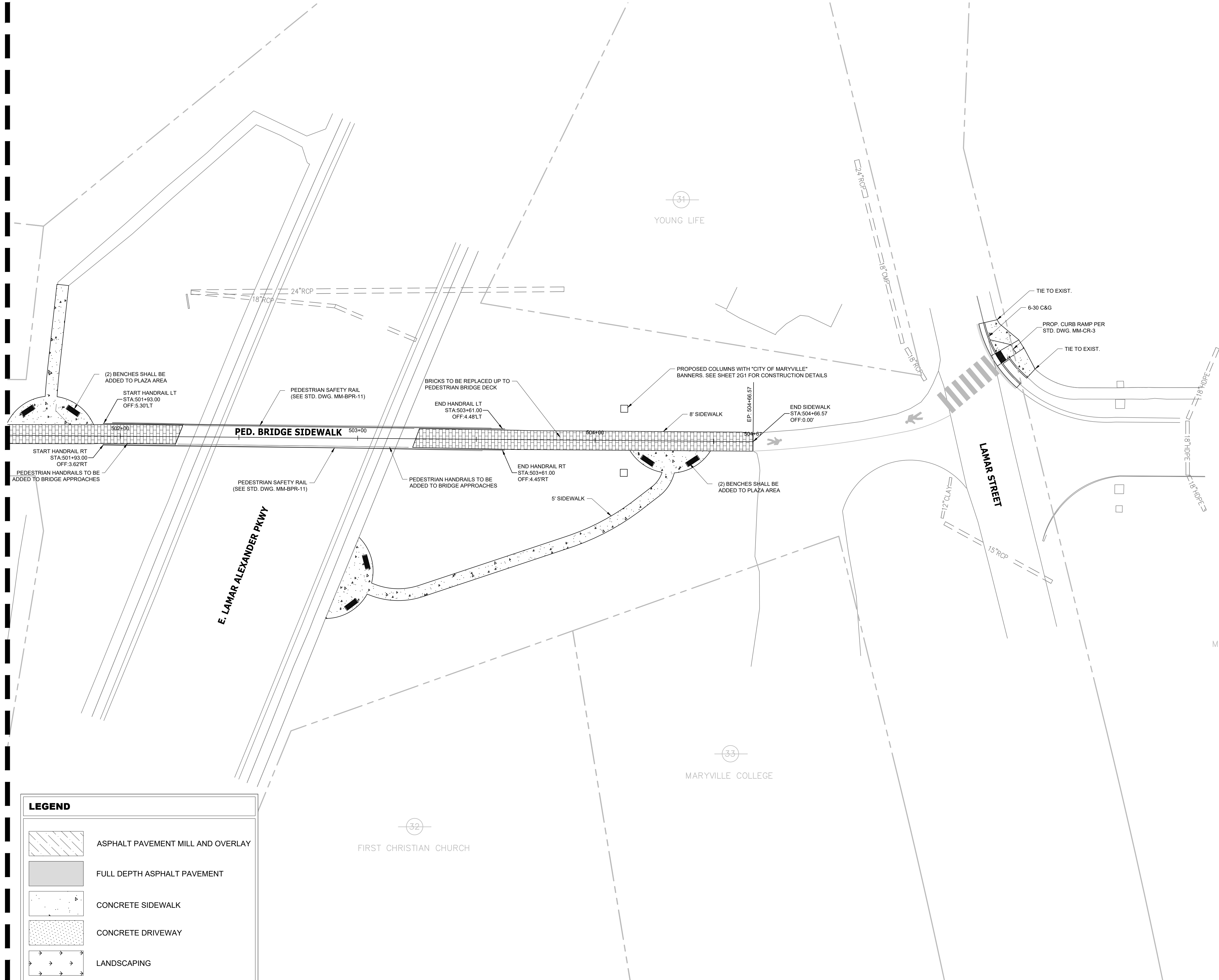
FULL DEPTH ASPHALT PAVEMENT

CONCRETE SIDEWALK

CONCRETE DRIVEWAY

LANDSCAPING

BRICK



COLLEGE STREET ROADWAY  
IMPROVEMENTS  
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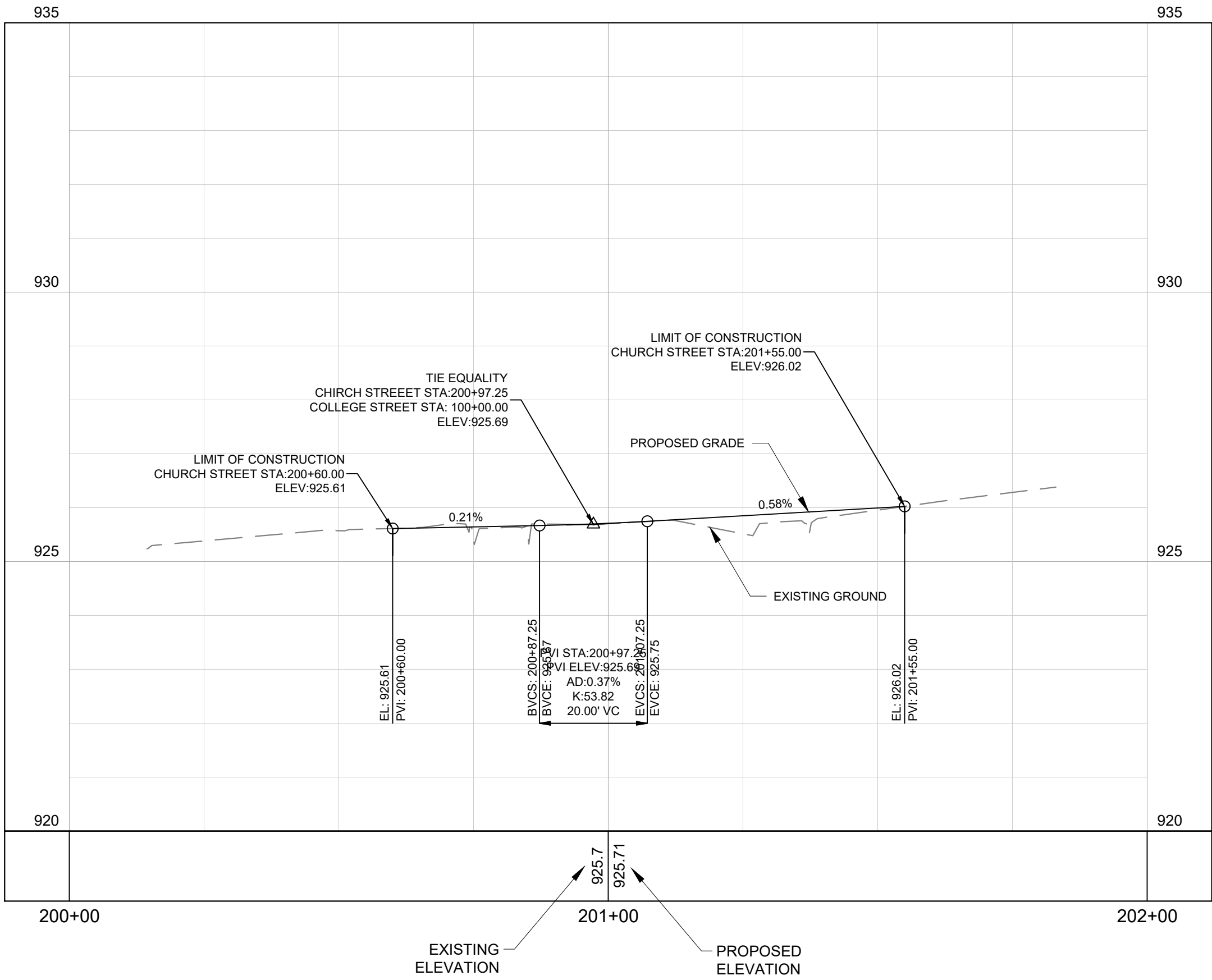
PROPOSED LAYOUT

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**6A**

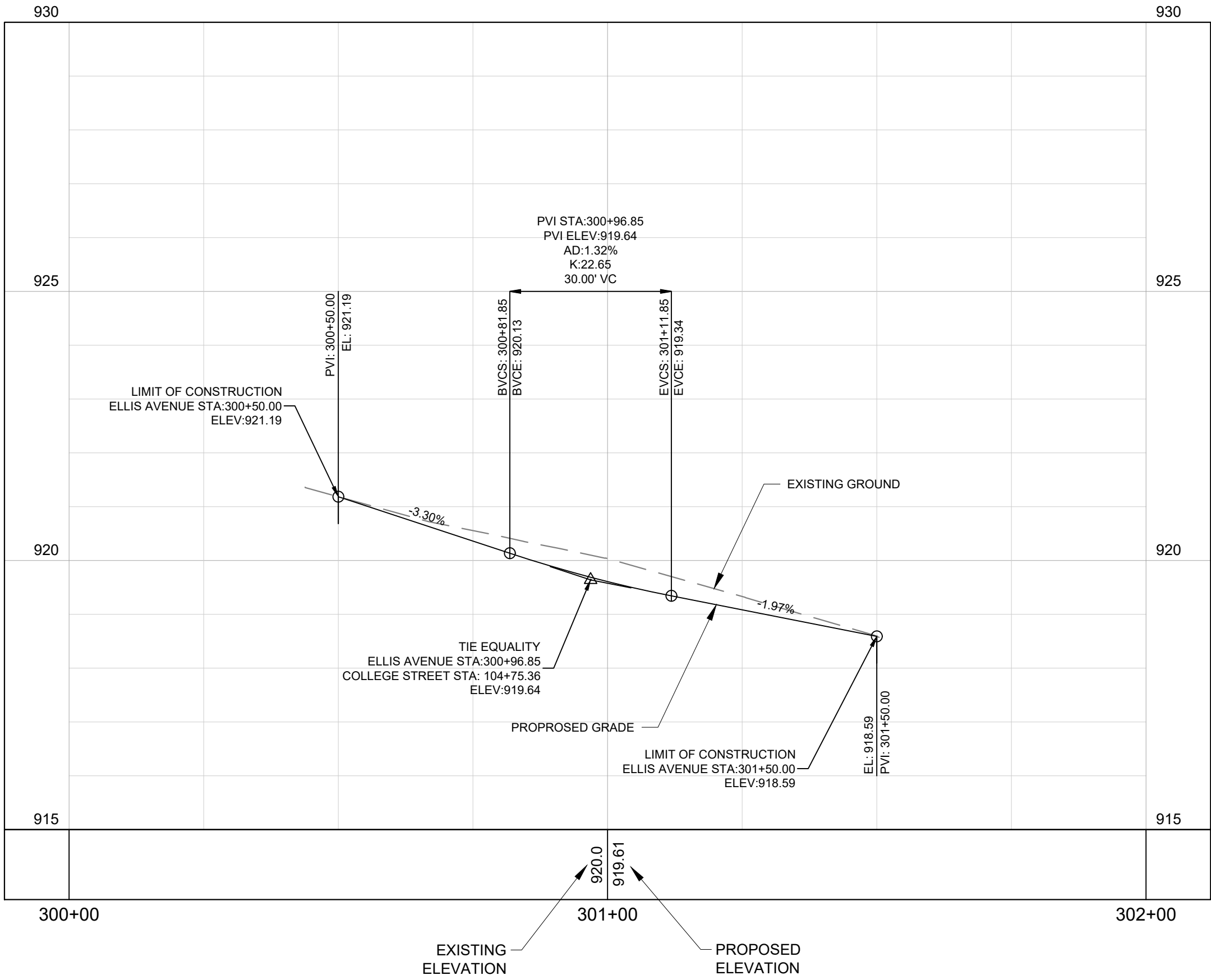
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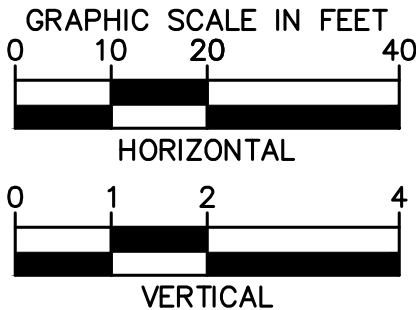
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E. CHURCH STREET



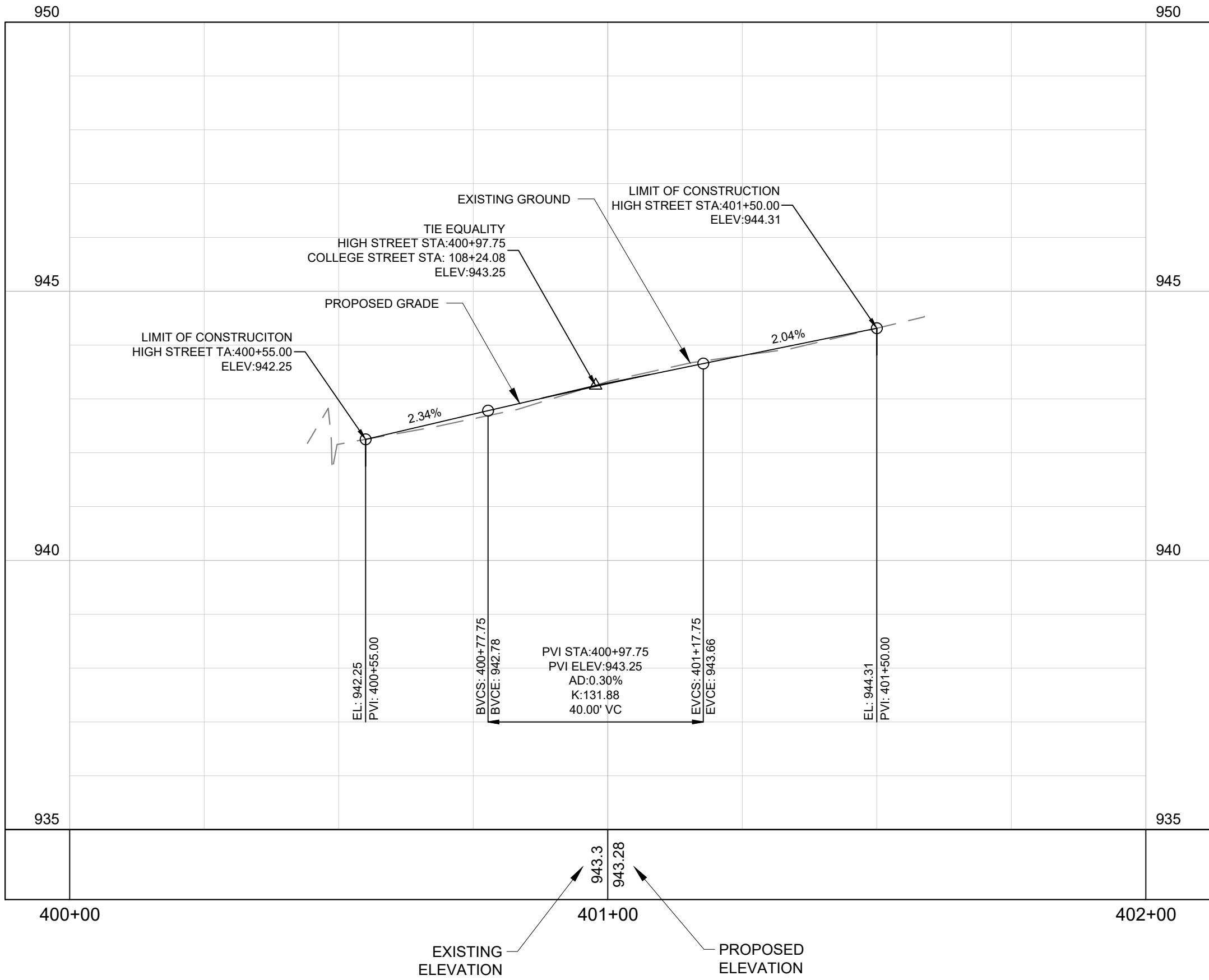
ELLIS AVENUE PROFILE



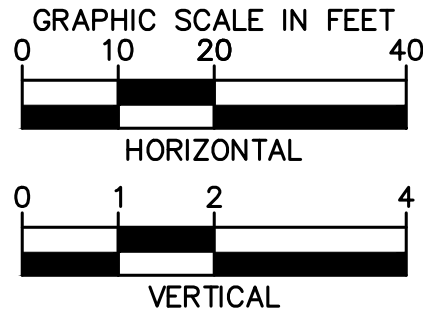
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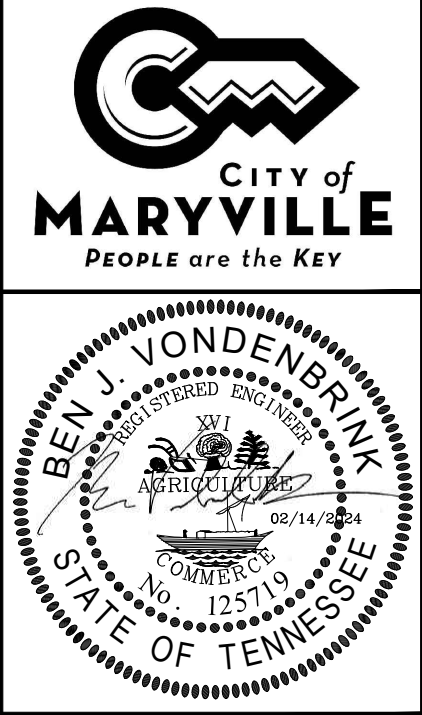
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## HIGH STREET PROFILE

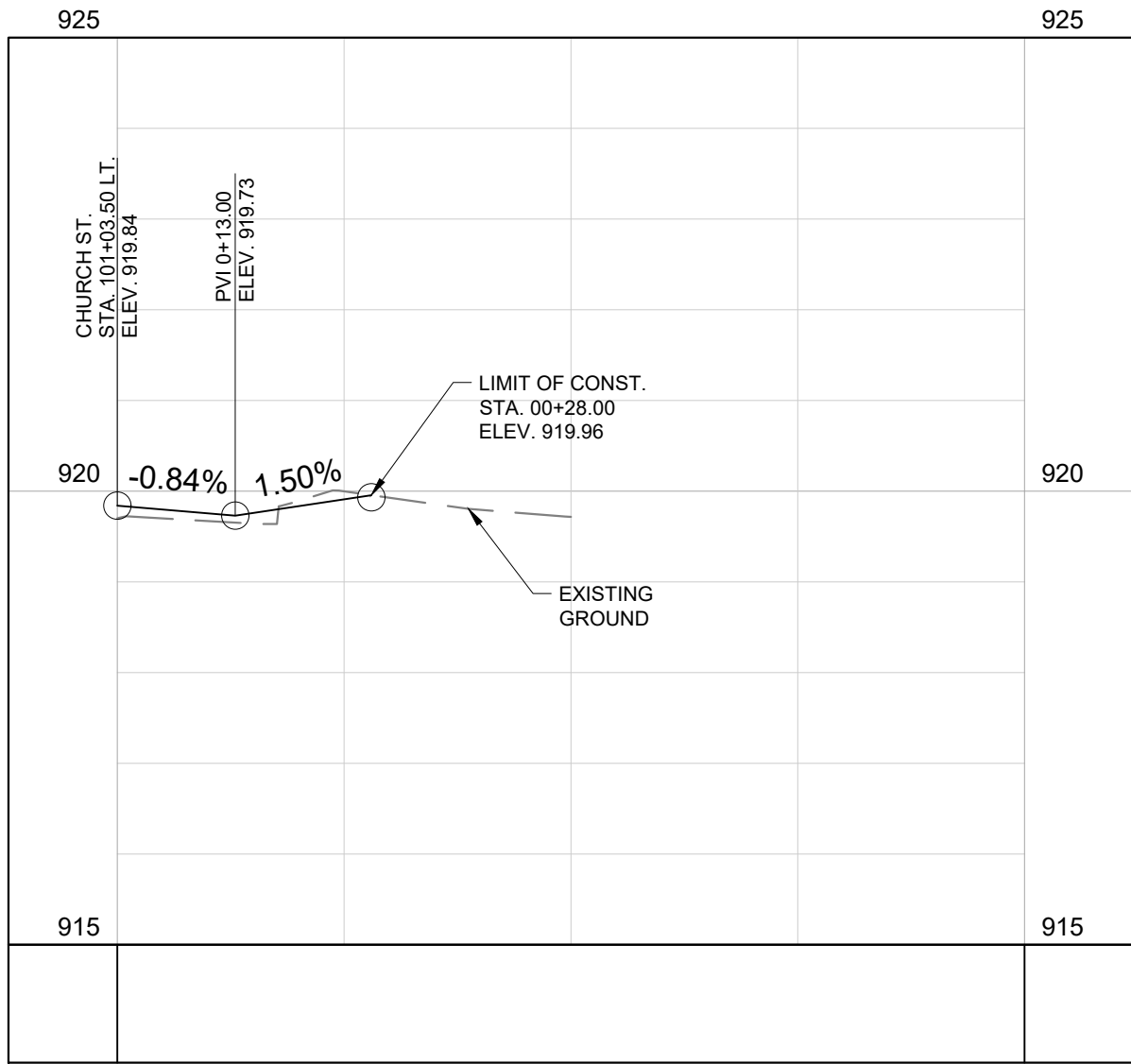


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SIDEROAD PROFILES											
SHEET NUMBER										8	

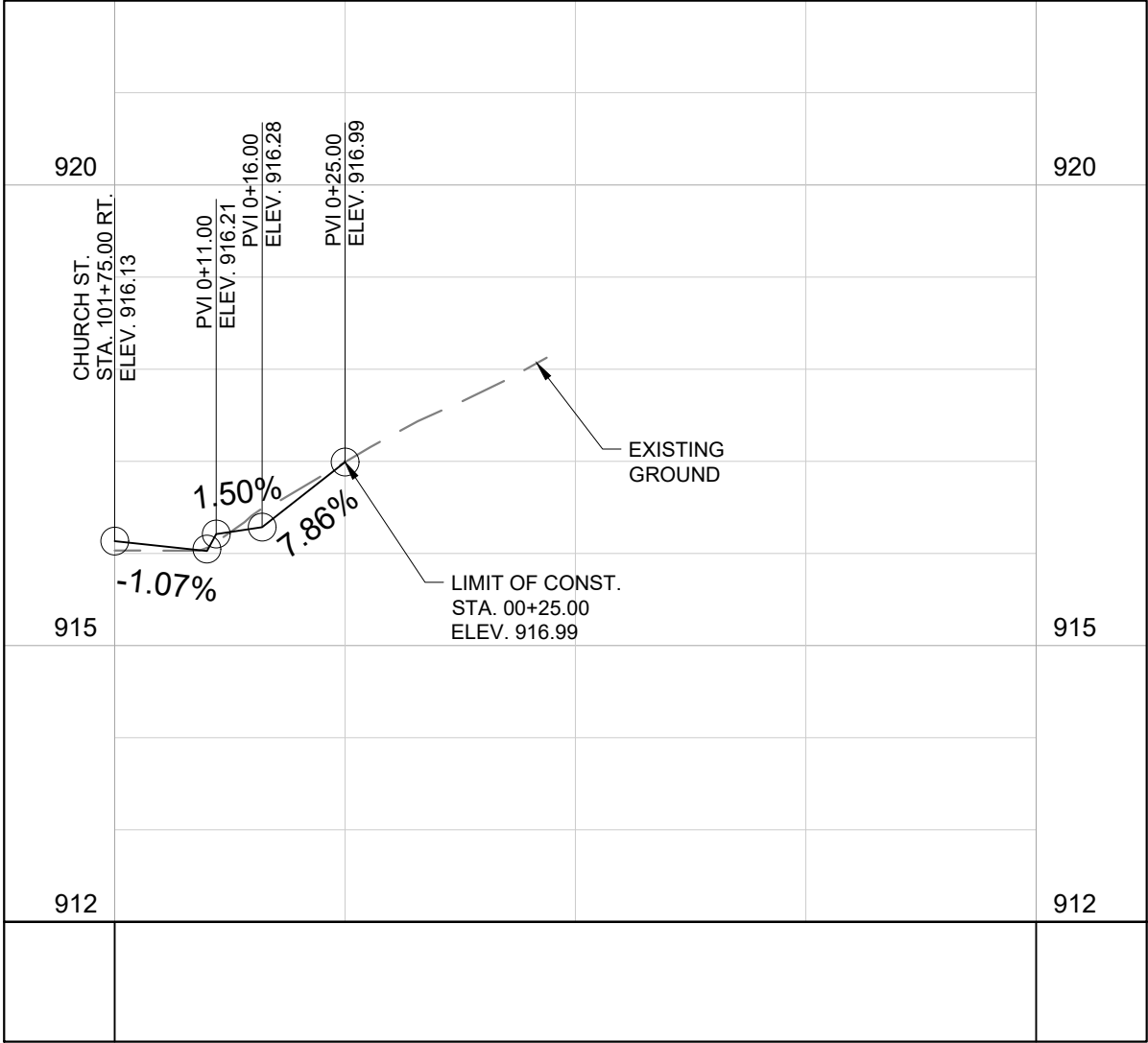


COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN

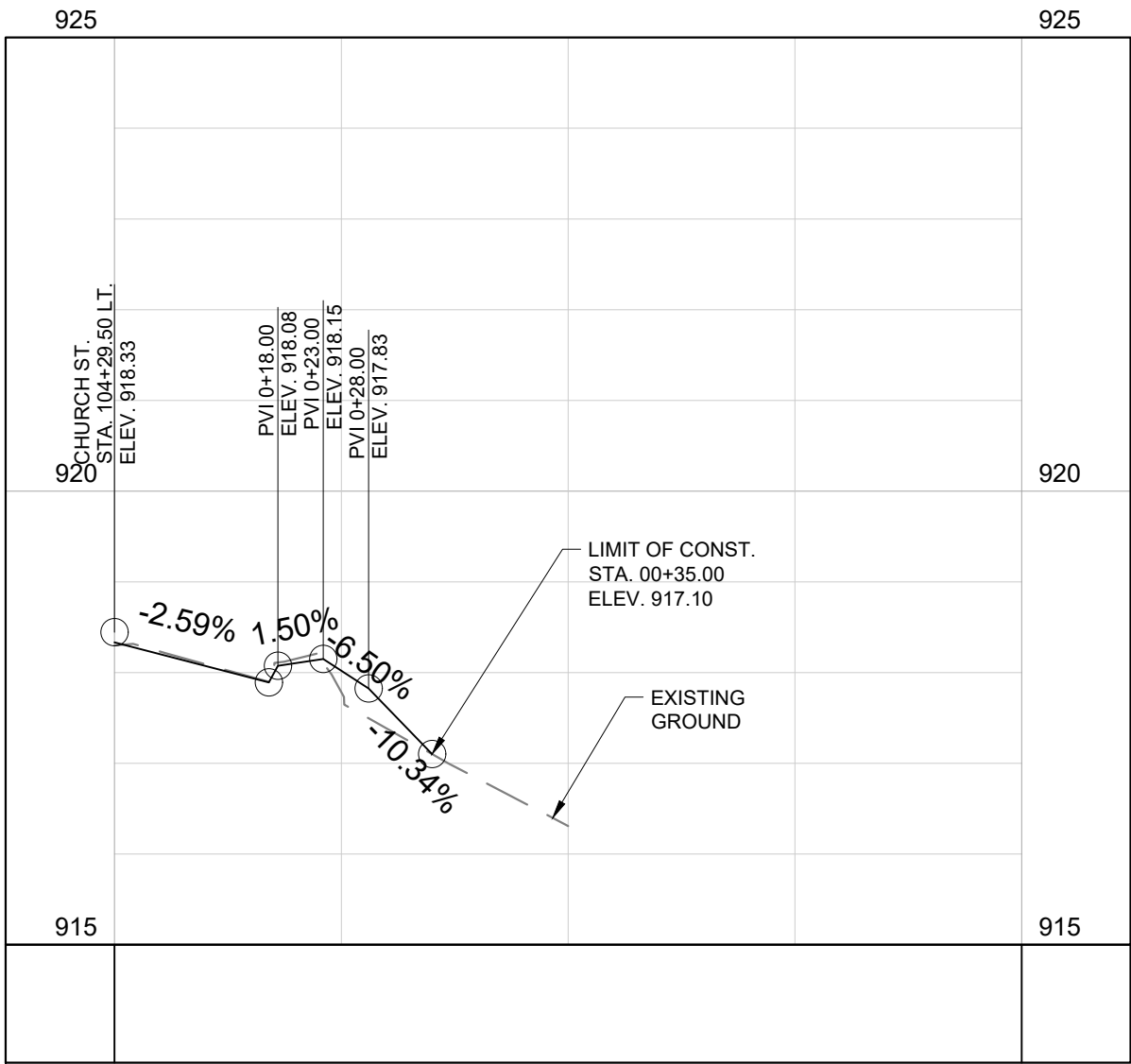
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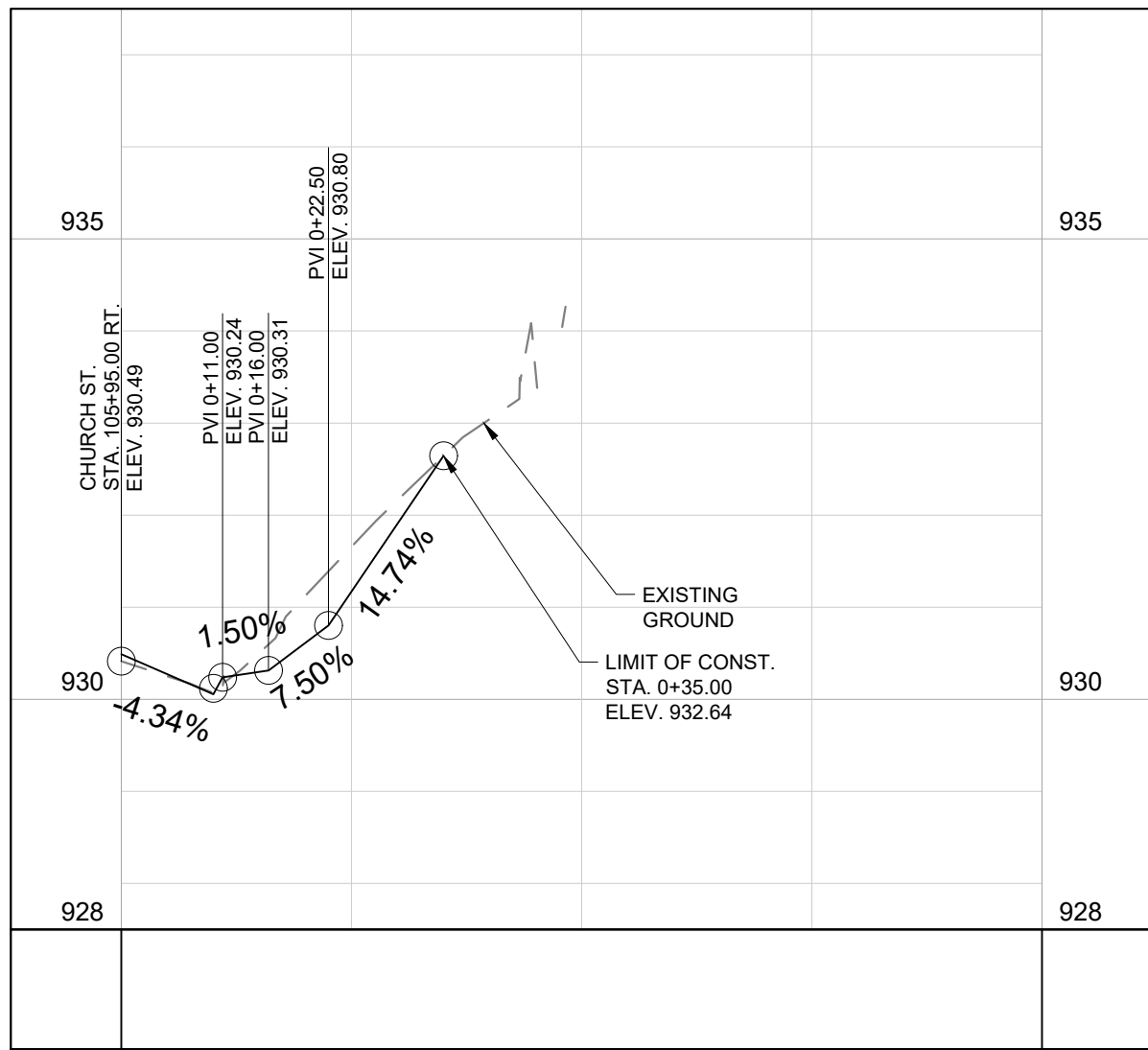
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COLLEGE STREET  
CONCRETE DRIVEWAY



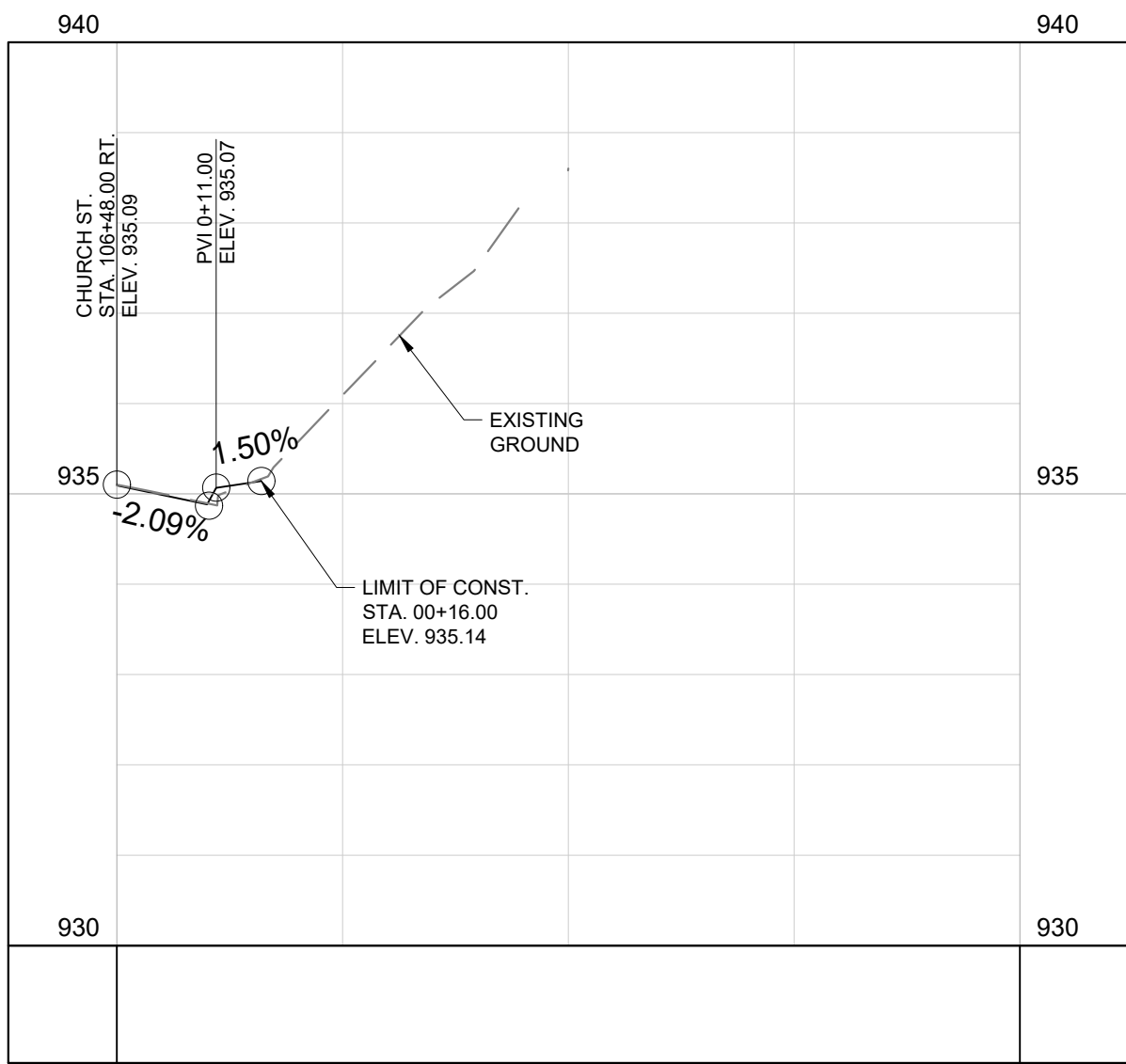
STA. 101+75.00 RT  
22' PVMT. DR.  
COLLEGE STREET  
ASPHALT DRIVEWAY



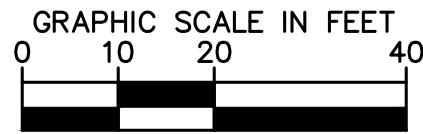
STA. 104+29.50 LT  
24' PVMT. DR.  
COLLEGE STREET  
CONCRETE DRIVEWAY



STA. 105+95.00 RT  
24' PVMT. DR.  
COLLEGE STREET  
CONCRETE DRIVEWAY



STA. 106+48.00 RT  
24' PVMT. DR.  
COLLEGE STREET  
CONCRETE DRIVEWAY



No.	REVISIONS									
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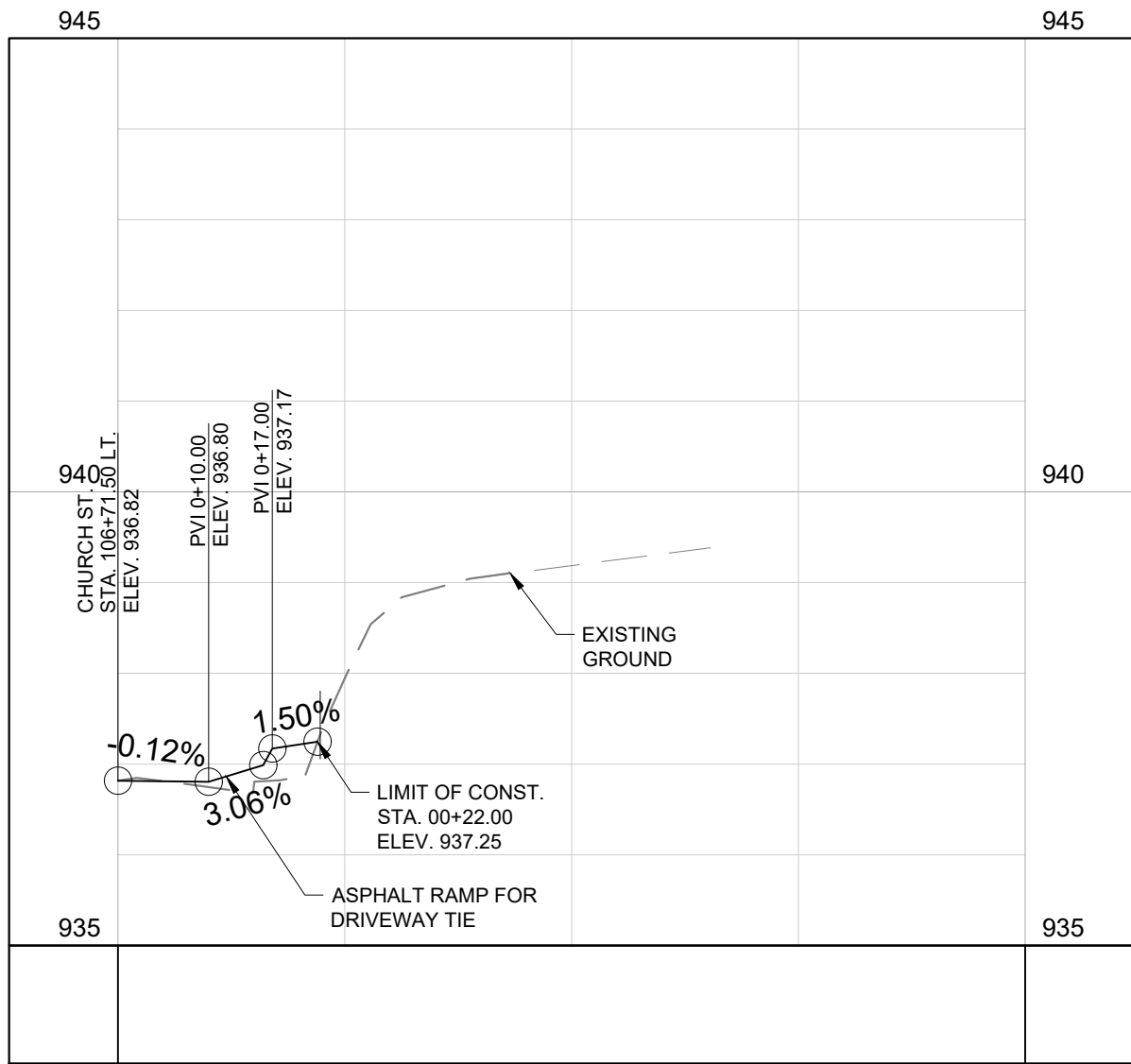
DESIGNED BY:	BJV
DRAWN BY:	EJBF
CHECKED BY:	DED
DATE:	02/14/2024
KIMLEY-HORN PROJECT NO. 118019006	

DRIVEWAY PROFILES

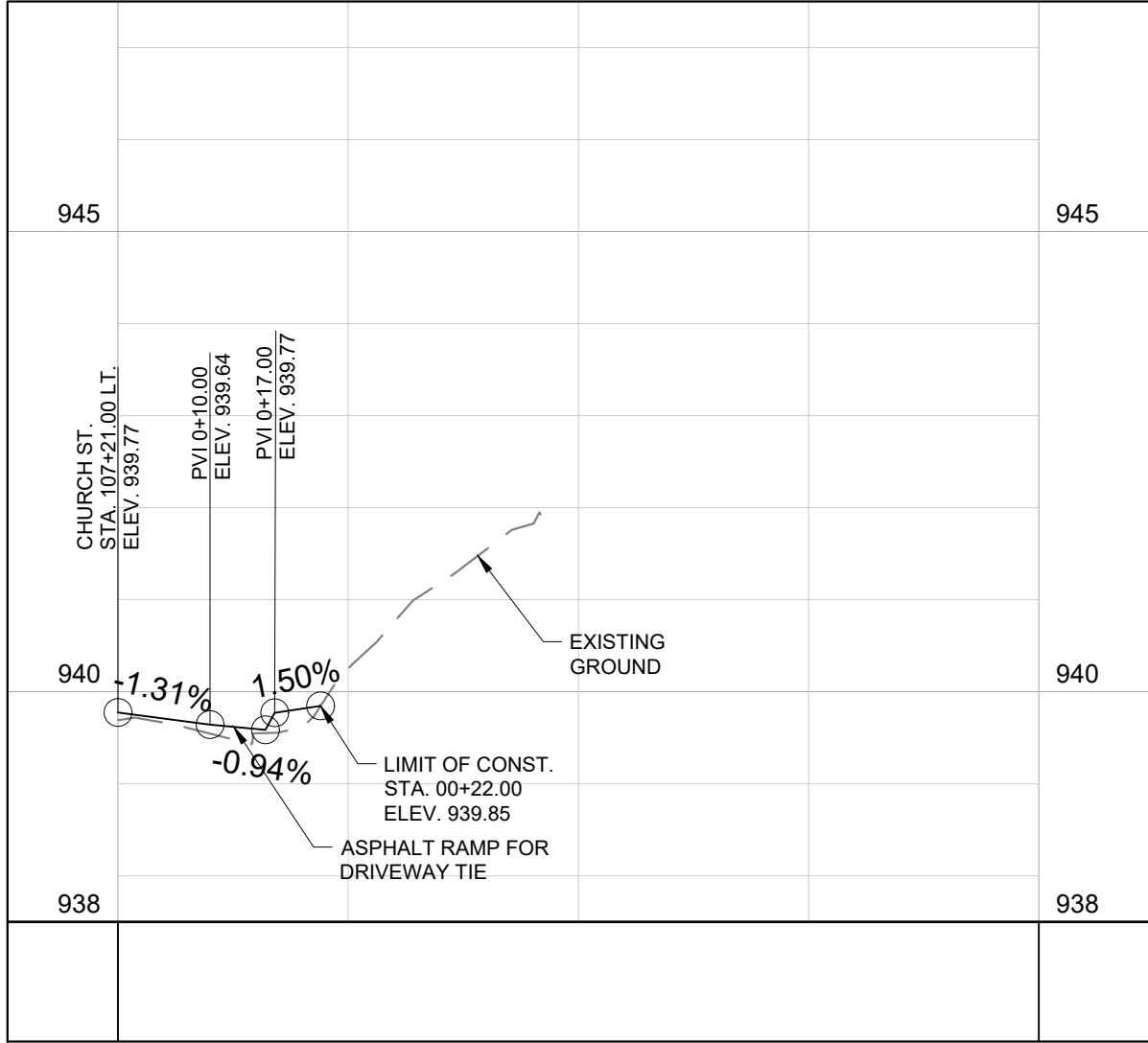
SHEET NUMBER

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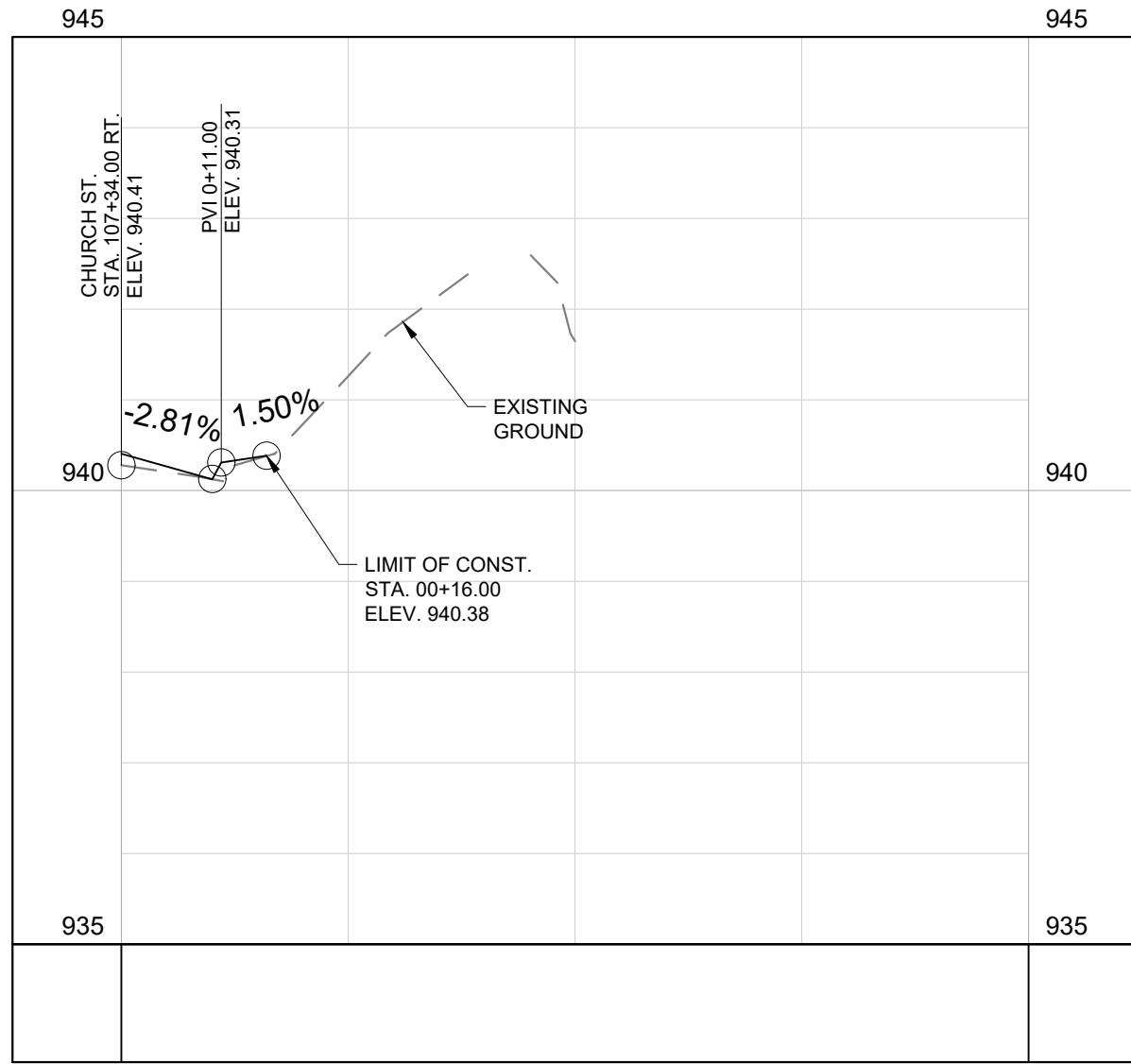
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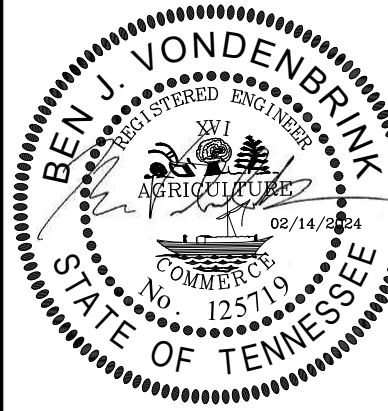
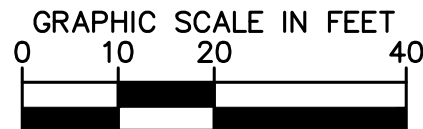
STA. 106+71.50 LT  
8' PVMT. DR.  
COLLEGE STREET  
CONCRETE DRIVEWAY



STA. 107+21.00 LT  
8' PVMT. DR.  
COLLEGE STREET  
ASPHALT DRIVEWAY



STA. 107+34.00 RT  
18' PVMT. DR.  
COLLEGE STREET  
ASPHALT DRIVEWAY



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DRIVEWAY PROFILES

SHEET NUMBER

10

COLLEGE STREET ROADWAY  
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CITY OF MARYVILLE  
MARYVILLE, TN

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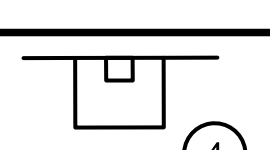
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## GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- ## UTILITY RELOCATION

- ## GENERAL NOTES

- | EROSION PREVENTION AND SEDIMENT CONTROL LEGEND                                      |                                   |            |
|---|-----------------------------------|------------|
| SYMBOL  | ITEM                              | STD. DWG.  |
|  | LIMITS OF DISTURBANCE             |            |
|  | SILT FENCE                        | EC-STR-3B  |
|  | DISTURBED AREA                    | N/A        |
|  | FILTER SOCK<br>(12 INCH)          | EC-STR-8   |
|  | CURB INLET PROTECTION<br>(TYPE 4) | EC-STR-39A |
- \*NOTES:
1. WHERE LOD LINSTYLES NOT SHOWN, SILT FENCE REPRESENTS LOD.
  2. USE "J" HOOKS WHERE SILT FENCE IS NOT ON CONTOUR.

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No.	1	2	3	4	5	6	7	8	9	10
DESIGNED BY:									BJV	
DRAWN BY:									EJBF	
CHECKED BY:									DED	
DATE:									02/14/2024	
KIMLEY-HORN PROJECT NO. 118019006										

EPSC NOTES

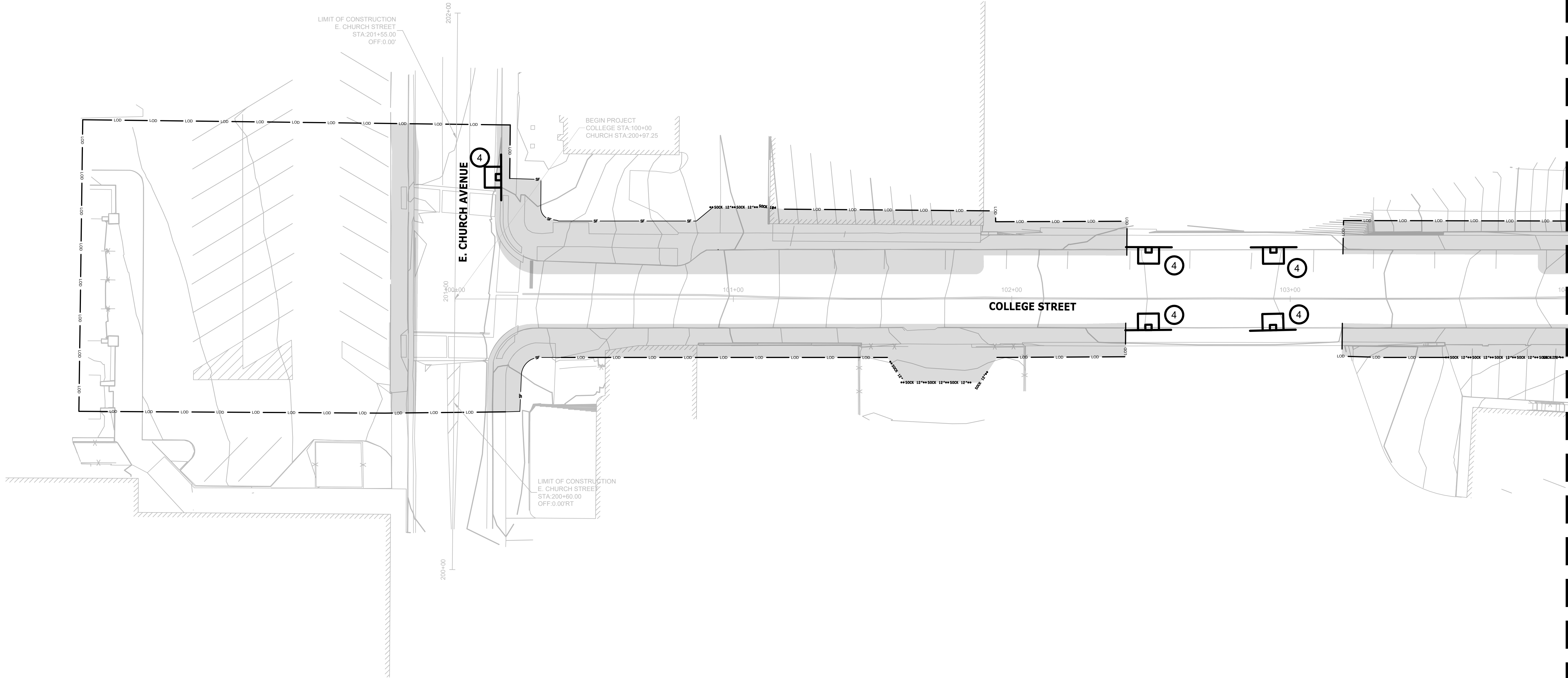
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EROSION PREVENTION AND  
SEDIMENT CONTROL LEGEND

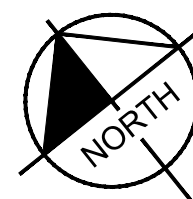
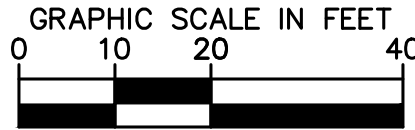
SYMBOL	ITEM	STD. DWG.
—LOD—LOD—LOD—	LIMITS OF DISTURBANCE	
* SF * SF * SF *	SILT FENCE	EC-STR-3B
	DISTURBED AREA	N/A
** SOCK 12 **	FILTER SOCK (12 INCH)	EC-STR-8
<div>4</div>	CURB INLET PROTECTION (TYPE 4)	EC-STR-39A

\*NOTES:  
1. WHERE LOD LINSTYLES NOT SHOWN, SILT FENCE REPRESENTS LOD.  
2. USE "J" HOOKS WHERE SILT FENCE IS NOT ON CONTOUR.

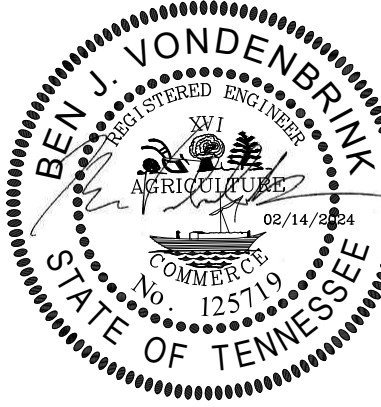


PHASE 1

MATCHLINE STA. 104+00 SEE SHEET 12A



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CITY OF MARYVILLE  
MARYVILLE, TN



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DESIGNED BY:		BJV	
DRAWN BY:		EJBF	
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DATE:		02/14/2024	
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EPSC STAGE 1

SHEET NUMBER

12

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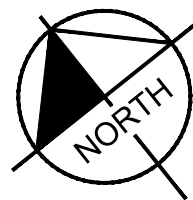
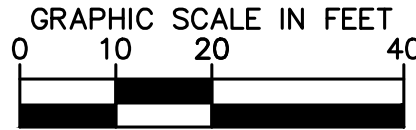
PHASE 1

MATCHLINE SEE SHEET 12B

EROSION PREVENTION AND  
SEDIMENT CONTROL LEGEND

SYMBOL	ITEM	STD. DWG.
—LOD—LOD—LOD—	LIMITS OF DISTURBANCE	
* SF * SF * SF *	SILT FENCE	EC-STR-3B
	DISTURBED AREA	N/A
** SOCK 12" **	FILTER SOCK (12 INCH)	EC-STR-8
<div>4</div>	CURB INLET PROTECTION (TYPE 4)	EC-STR-39A

\*NOTES:  
1. WHERE LOD LINSTYLES NOT SHOWN, SILT FENCE REPRESENTS LOD.  
2. USE "J" HOOKS WHERE SILT FENCE IS NOT ON CONTOUR.



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REVISIONS										DATE	BY
No.	1	2	3	4	5	6	7	8	9	10	
DESIGNED BY:											BJV
DRAWN BY:											EJBF
CHECKED BY:											DED
DATE:										02/14/2024	
KIMLEY-HORN PROJECT NO.										118019006	

EPSC STAGE 1

SHEET NUMBER  
12A



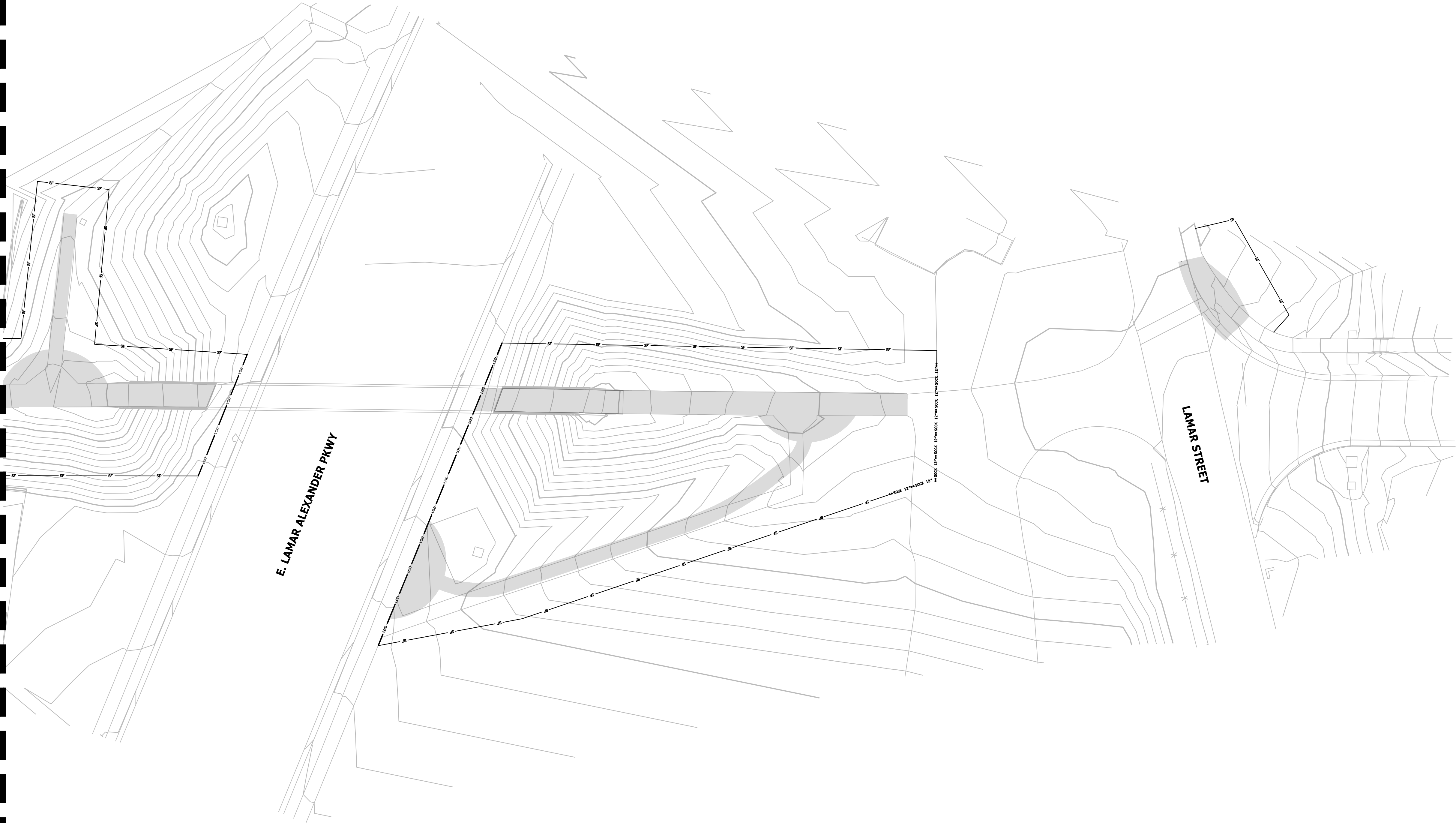
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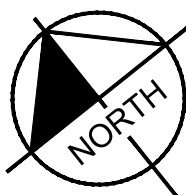
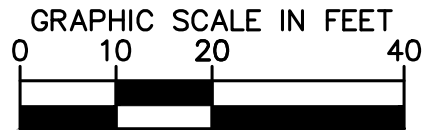
EROSION PREVENTION AND  
SEDIMENT CONTROL LEGEND

SYMBOL	ITEM	STD. DWG.
—LOD—LOD—LOD—	LIMITS OF DISTURBANCE	
* SF * SF * SF *	SILT FENCE	EC-STR-3B
	DISTURBED AREA	N/A
** SOCK 12***	FILTER SOCK (12 INCH)	EC-STR-8

\*NOTES:  
1. WHERE LOD LINSTYLES NOT SHOWN, SILT FENCE REPRESENTS LOD.  
2. USE "J" HOOKS WHERE SILT FENCE IS NOT ON CONTOUR.



PHASE 1



COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN



REVISIONS										DATE	BY
No.	1	2	3	4	5	6	7	8	9	10	
DESIGNED BY:											B.VJ
DRAWN BY:											E.JBF
CHECKED BY:											DED
DATE:										02/14/2024	
KIMLEY-HORN PROJECT NO.										118019006	

EPSC STAGE 1

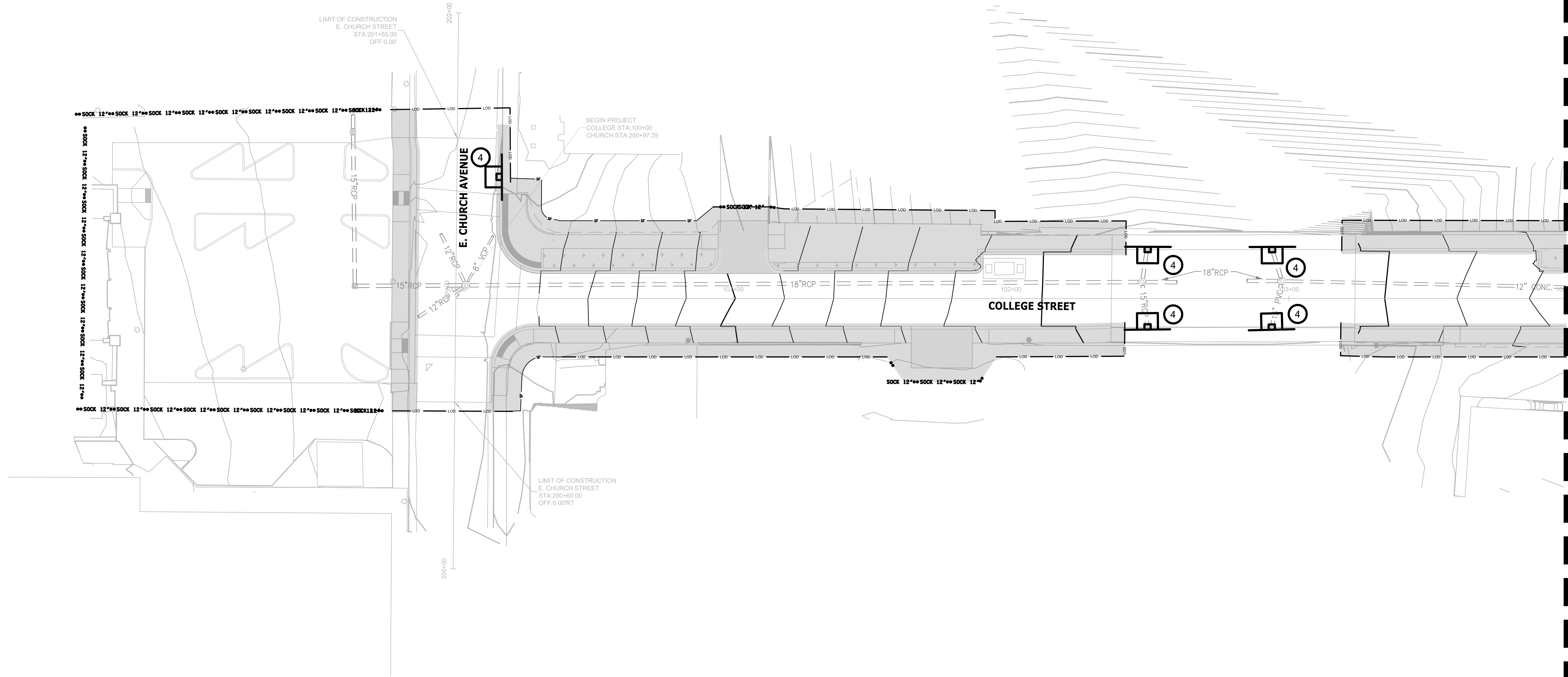
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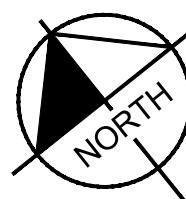
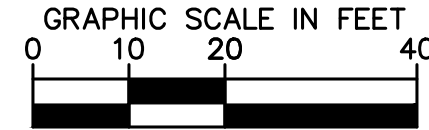
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EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
—LOD—LOD—LOD—	LIMITS OF DISTURBANCE	
* SF * SF * SF *	SILT FENCE	EC-STR-3B
	DISTURBED AREA	N/A
** SOCK 12" **	FILTER SOCK (12 INCH)	EC-STR-8
<div><div></div><div>4</div></div>	CURB INLET PROTECTION (TYPE 4)	EC-STR-39A
*NOTES: 1. WHERE LOD LINSTYLES NOT SHOWN, SILT FENCE REPRESENTS LOD. 2. USE "J" HOOKS WHERE SILT FENCE IS NOT ON CONTOUR.		



PHASE 2

MATCHLINE STA. 104+00 SEE SHEET 13A



COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN



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DRAWN BY:		EJBF	
CHECKED BY:		DED	
DATE:		02/14/2024	
KIMLEY-HORN PROJECT NO.		118019006	

EPSC STAGE 2

SHEET NUMBER

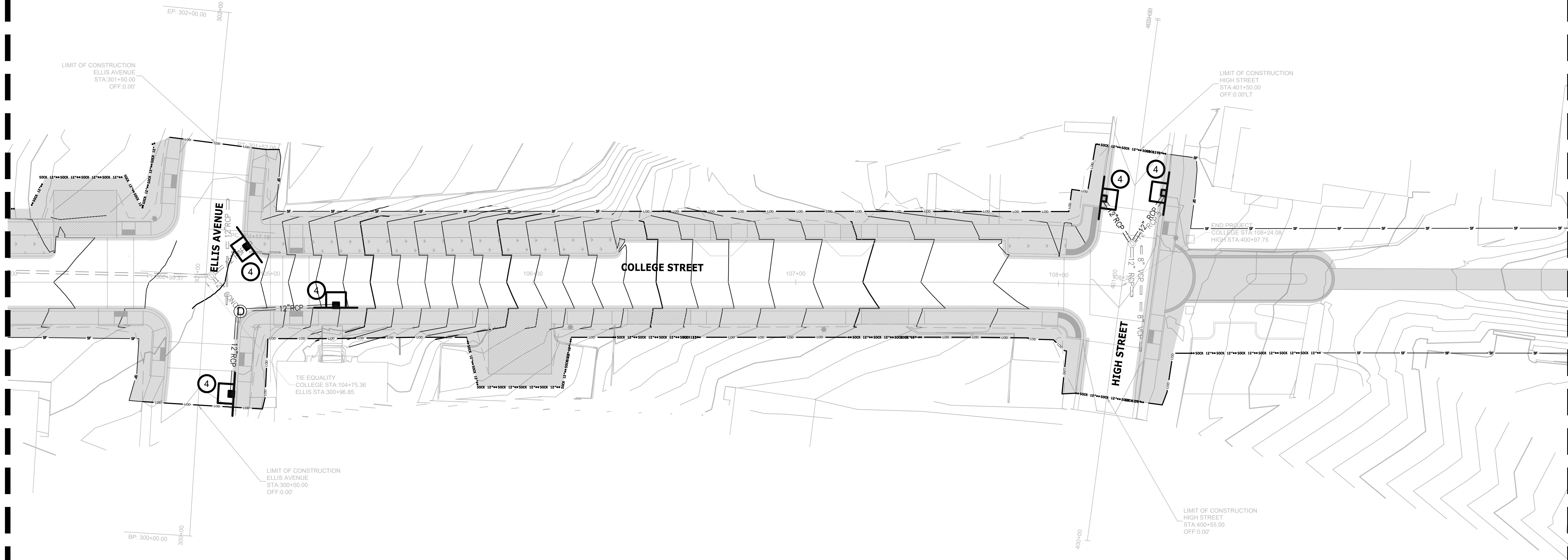
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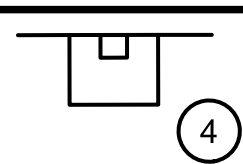
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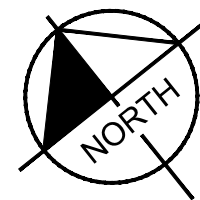
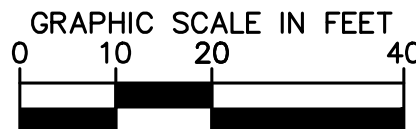
MATCHLINE STA. 104+00 SEE SHEET 13



EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
—LOD—LOD—LOD—	LIMITS OF DISTURBANCE	
* SF * SF * SF *	SILT FENCE	EC-STR-3B
	DISTURBED AREA	N/A
** SOCK 12" **	FILTER SOCK (12 INCH)	EC-STR-8
	CURB INLET PROTECTION (TYPE 4)	EC-STR-39A
*NOTES: 1. WHERE LOD LINSTYLES NOT SHOWN, SILT FENCE REPRESENTS LOD. 2. USE "J" HOOKS WHERE SILT FENCE IS NOT ON CONTOUR.		

PHASE 2

MATCHLINE SEE SHEET 13B



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COLLEGE STREET ROADWAY  
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DESIGNED BY:		BJ	
DRAWN BY:		EJB	
CHECKED BY:		DEI	
DATE:		02/14/2024	
KIMLEY-HORN PROJECT NO. 118019006			

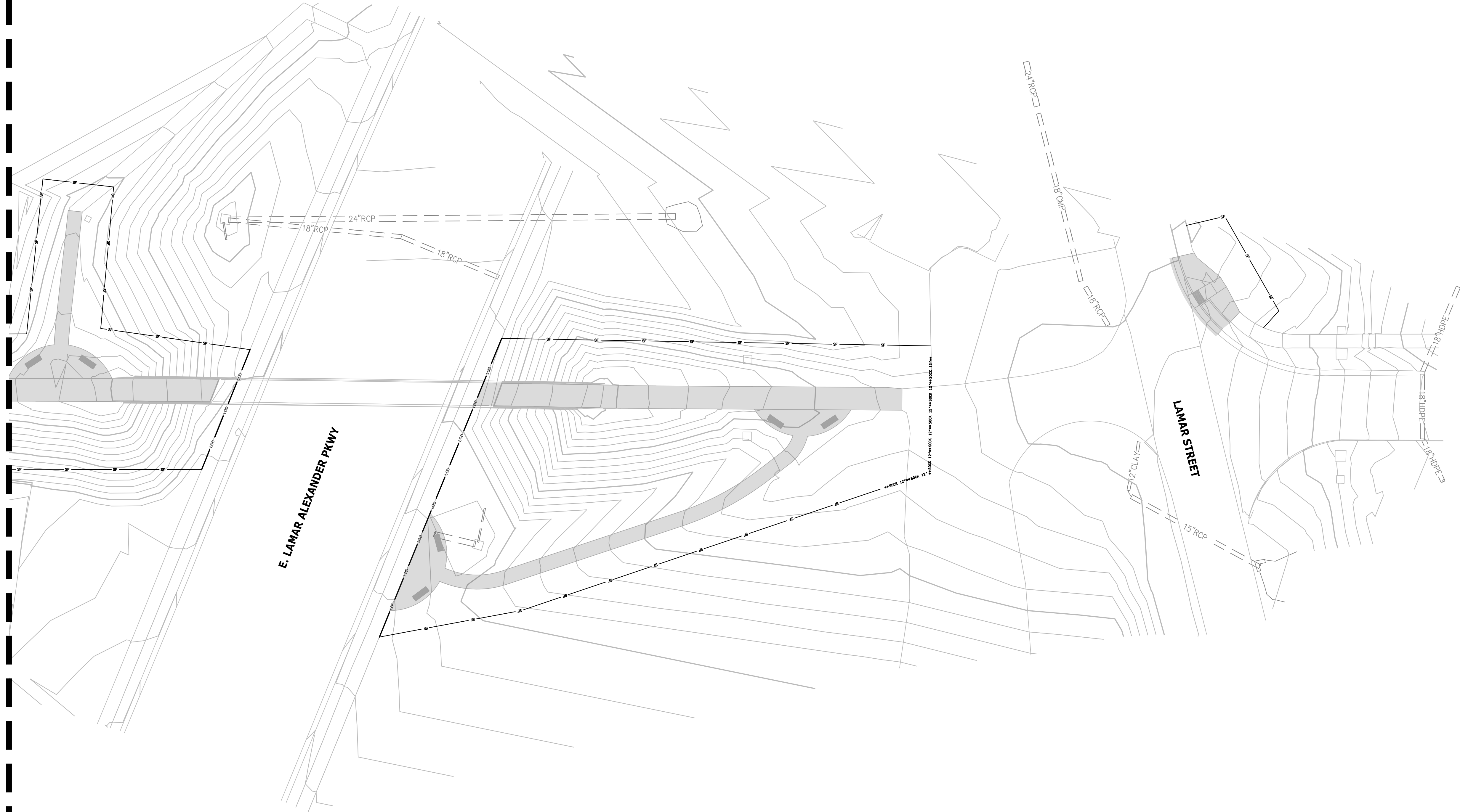
EPSC STAGE 2

SHEET NUMBER  
13A



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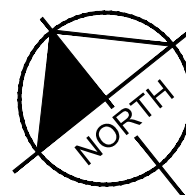
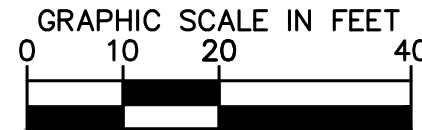
MATCHLINE SEE SHEET 13A



EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
—LOD—LOD—LOD—	LIMITS OF DISTURBANCE	
* SF * SF * SF *	SILT FENCE	EC-STR-3B
	DISTURBED AREA	N/A
** SOCK 12***	FILTER SOCK (12 INCH)	EC-STR-8

\*NOTES:  
1. WHERE LOD LINSTYLES NOT SHOWN, SILT FENCE REPRESENTS LOD.  
2. USE "J" HOOKS WHERE SILT FENCE IS NOT ON CONTOUR.

PHASE 2



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COLLEGE STREET ROADWAY IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN

CITY of MARYVILLE  
PEOPLE are the KEY

REVISIONS	DATE	BY								
No.	1	2	3	4	5	6	7	8	9	10

DESIGNED BY: BJV

DRAWN BY: EJB

CHECKED BY: DED

DATE: 02/14/2024

KIMLEY-HORN PROJECT NO. 118019006

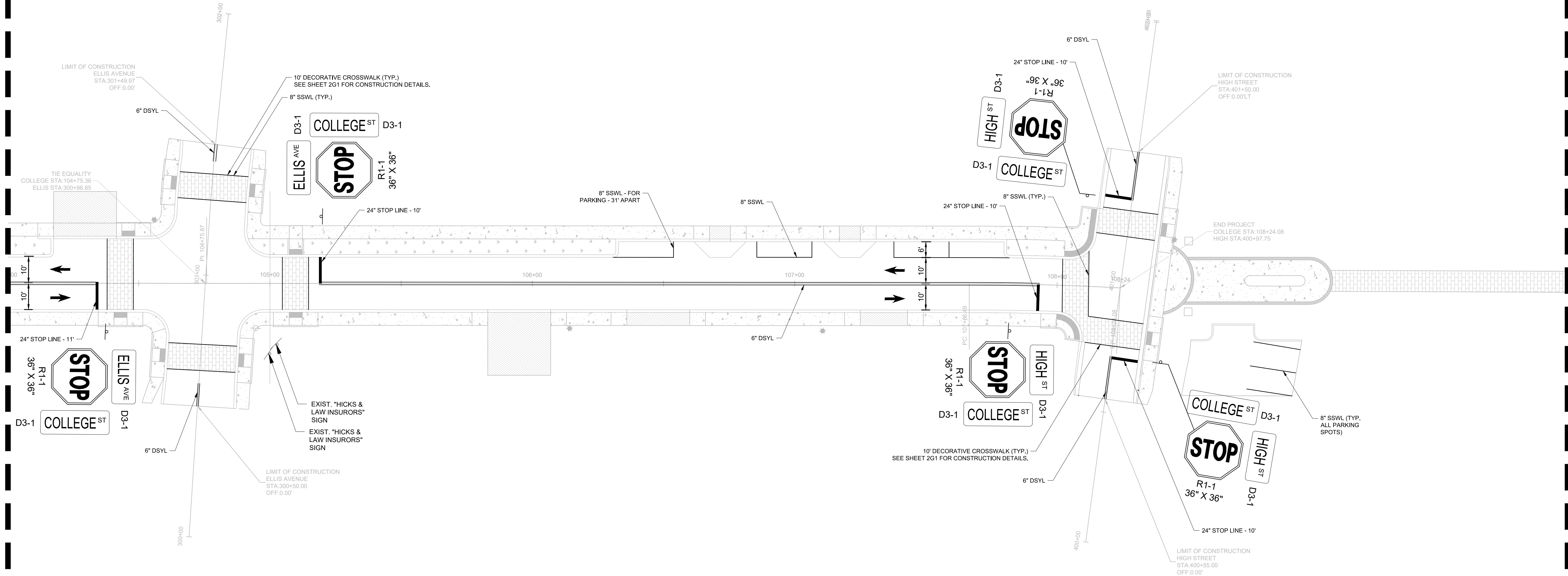
EPSC STAGE 2

SHEET NUMBER 13B

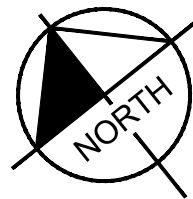
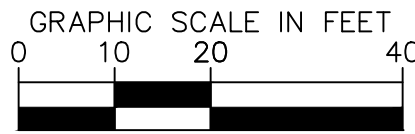


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MATCHLINE STA. 104+00 SEE SHEET 14



MATCHLINE SEE SHEET 16



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DESIGNED BY:	BJV
DRAWN BY:	EJBF
CHECKED BY:	DED
DATE:	02/14/2024
KIMLEY-HORN PROJECT NO. 118019006	

SIGNING AND MARKING

SHEET NUMBER  
15

COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN

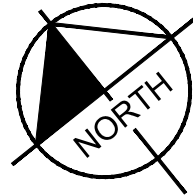
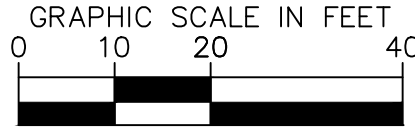
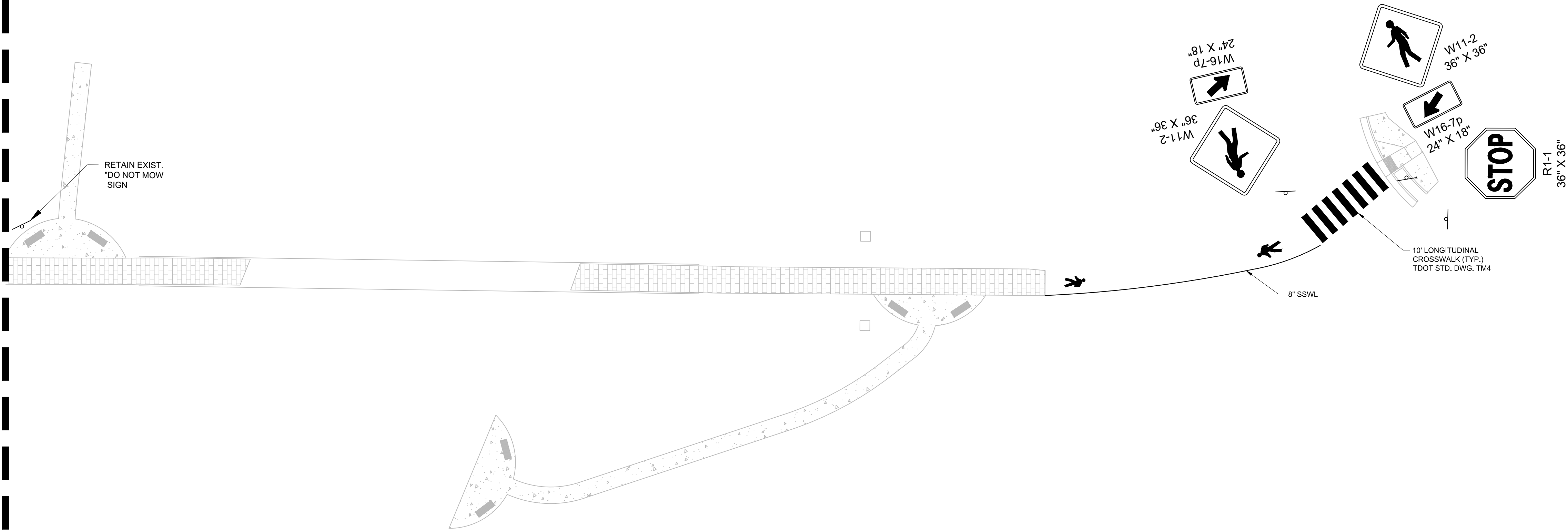
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MATCHLINE SEE SHEET 12A



REVISIONS		DATE	BY
No.	1		
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DESIGNED BY:		BJV	
DRAWN BY:		EJBF	
CHECKED BY:		DED	
DATE:		02/14/2024	
KIMLEY-HORN PROJECT NO.		118019006	

SIGNING AND MARKING

SHEET NUMBER

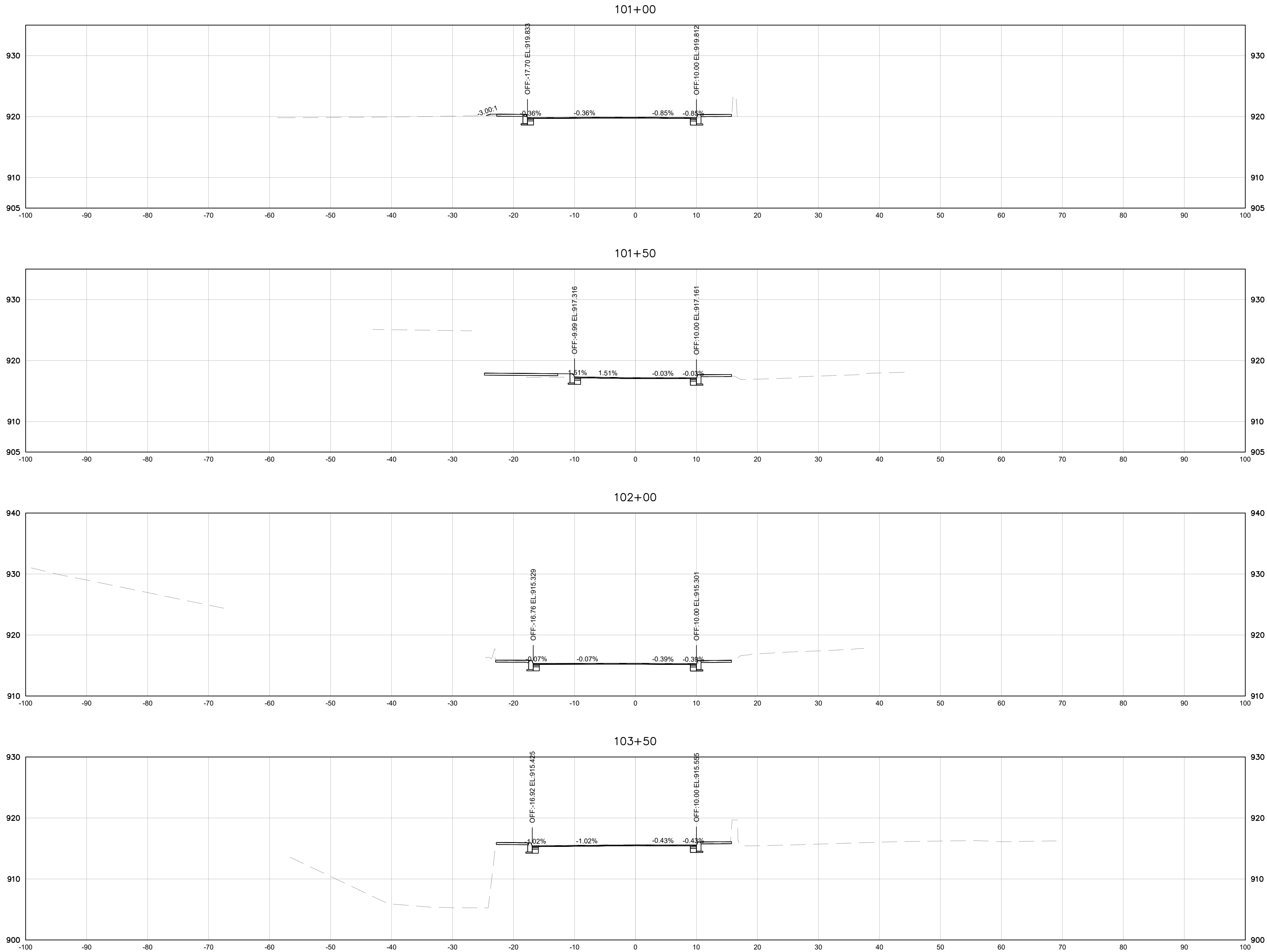
16

COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN

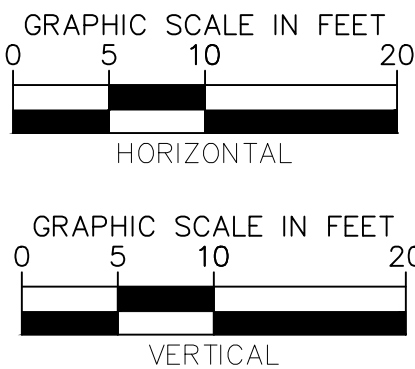


REVISIONS		DATE	BY
No.	1		
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	10		
DESIGNED BY:		BJV	
DRAWN BY:		EJBF	
CHECKED BY:		DED	
DATE:		02/14/2024	
KIMLEY-HORN PROJECT NO.		118019006	

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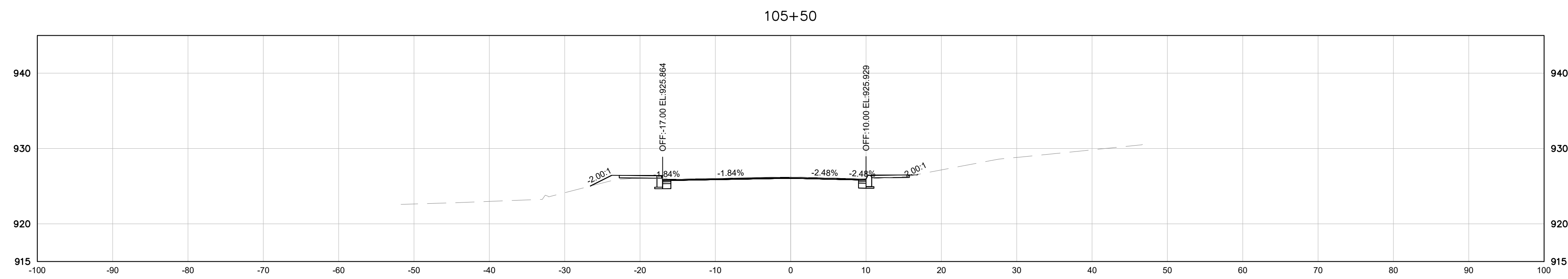
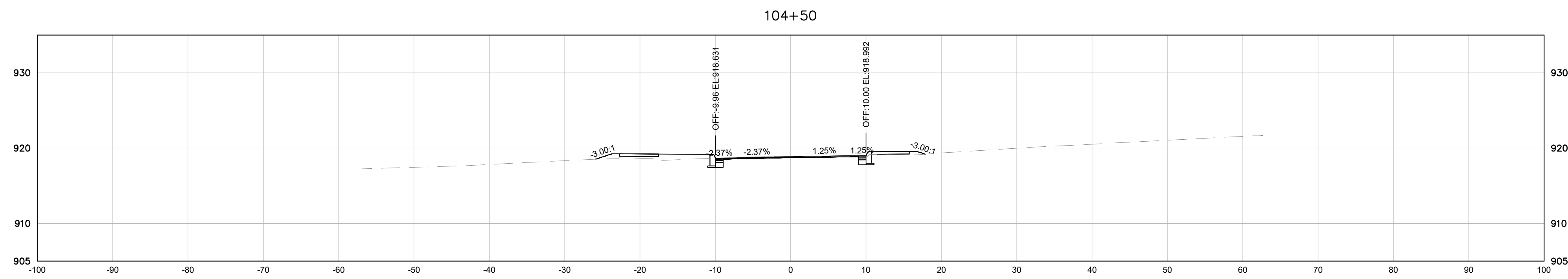


COLLEGE STREET



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DESIGNED BY:		BJV								
DRAWN BY:		EJBF								
CHECKED BY:		DED								
DATE:		02/14/2024								
KIMLEY-HORN PROJECT NO.		118019006								

Drawing name: K:\NSH\_Roadway\118019006 - Maryville Streetscape\Cadd\Plans\017-019 - Cross Sections.dwg 18 - CROSS SECTION Feb 14, 2024 1:33pm by: Ben.Vondenbrink



GRAPHIC SCALE IN FEET

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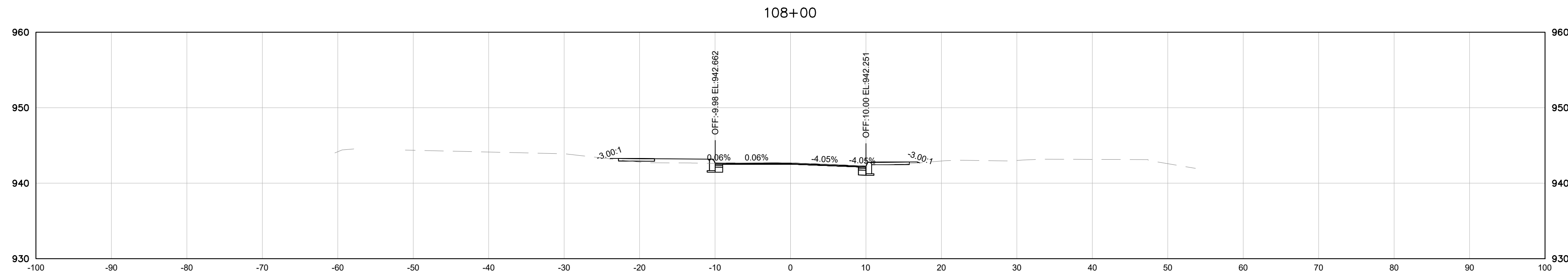
GRAPHIC SCALE IN FEET

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VERTICAL



Drawing name: K:\NSH\_Roadway\118019006 - Maryville Streetscape\Cadd\Plans\017-019 - Cross Sections.dwg 19 - CROSS SECTION Feb 14, 2024 1:33pm by: Ben.Vondenbrink



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PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.
- THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.
- C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:
- SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.
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- COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN
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| DESIGNED BY:            |     | BJV        |    |
| DRAWN BY:               |     | EJBF       |    |
| CHECKED BY:             |     | DED        |    |
| DATE:                   |     | 02/14/2024 |    |
| KIMLEY-HORN PROJECT NO. |     | 118019006  |    |
- PAVEMENT EDGE DROP  
OFF NOTES
- SHEET NUMBER  
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- Drawing name: K:\NSH\_Roadway\118019006 - Maryville Streetscape\Cadd\Plans\T-1 - Pavement Drop Off.dwg PAVEMENT DROP OFF Feb 14, 2024 1:33pm by: Ben.Vondenbrink

TRAFFIC CONTROL NOTES

1.

SIGNS SHOWN ON THIS PLAN ARE TO WARN TRAFFIC ABOUT THE CONSTRUCTION. OTHER TRAFFIC CONTROL DEVICES MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
2.

NOTHING IN THIS PLAN IS INTENDED TO SUPERSEDE OR RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING THE APPROPRIATE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS."
3.

WORK WITHIN THE ROADWAY DURING DIFFERENT PHASES OF CONSTRUCTION SHALL NOT BE ACTIVE AT THE SAME TIME.
4.

ONLY ONE PHASE OF THE TRAFFIC CONTROL PLAN SHALL BE ACTIVE AT ANY ONE TIME.
5.

ALL TEMPORARY OR PERMANENT TRAVELED SURFACES SHALL BE INSPECTED DAILY BY THE CONTRACTOR (INCLUDING WEEKENDS) AND NECESSARY PATCHING OR REFINISHING PERFORMED.
6.

MARKINGS SHALL BE MAINTAINED IN LONG-TERM STATIONARY WORK AREAS AND SHALL MATCH AND MEET THE MARKINGS IN PLACE AT BOTH ENDS OF THE WORK AREA.
7.

CENTERLINE/LANE LINES SHOULD BE PLACED REPLACED OR DELINEATED WHERE APPROPRIATE BEFORE THE ROADWAY IS OPENED TO TRAFFIC.
8.

THE APPROPRIATE TRAFFIC CONTROL SHALL BE INSTALLED AT THE INCEPTION OF EACH STAGE OF CONSTRUCTION AND SHALL BE PROPERLY MAINTAINED AND/OR OPERATED DURING THE TIME SUCH SPECIAL CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS THEY ARE NEEDED AND SHALL BE IMMEDIATELY REMOVED THEREAFTER.
9.

OBLITERATED MARKINGS SHALL BE UNIDENTIFIABLE AS PAVEMENT MARKINGS UNDER DAY OR NIGHT, WET OR DRY CONDITIONS. OVERLAYING EXISTING STRIPS WITH BLACK PAINT OR ASPHALT DOES NOT MEET THE REQUIREMENTS OF COVERING, REMOVAL, OR OBLITERATION; HOWEVER, THE USE OF REMOVABLE, NONREFLECTIVE, PREFORMED TAPE IS PERMITTED WHERE MARKINGS NEED TO BE COVERED TEMPORARILY.
10.

AT NO TIME SHALL ONE LANE SECTIONS BE LEFT OPEN UNATTENDED. WHERE TWO FLAGGERS ARE REQUIRED AND IN CIRCUMSTANCES WHERE ONLY ONE LANE OF TRAFFIC IS OPEN AND THERE IS NO CLEAR LINE OF SIGHT FROM ONE END OF THE CONSTRUCTION AREA TO THE OTHER, THE CONTRACTOR SHALL PROVIDE RADIO COMMUNICATION OR OTHER APPROPRIATE MEANS OF ESTABLISHING CONTROL OF TRAFFIC.
11.

ANY TIME A FLAGGER IS NOT PRESENT TO CONTROL TRAFFIC, TWO TRAFFIC LANES MUST BE OPEN TO MAINTAIN TWO-WAY TRAFFIC AND ALL INAPPROPRIATE SIGNS SHALL BE COVERED OR REMOVED.
12.

ALL FLAGGERS SHALL BE EQUIPPED WITH A STOP/SLOW PADDLE.
13.

REMOVE EXISTING PAVEMENT MARKINGS IN CONFLICT WITH TRAFFIC CONTROL PHASING.

TRAFFIC CONTROL PHASING NOTES

CONTRACTOR TO MAINTAIN TRAFFIC THROUGHOUT CONSTRUCTION UTILIZING AN APPROPRIATE TRAFFIC CONTROL PLAN IN ACCORDANCE WITH THE TDOT WORK ZONE FIELD MANUAL AND MUTCD STANDARDS.

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COLLEGE STREET ROADWAY  
IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN



REVISIONS										DATE	BY
No.	1	2	3	4	5	6	7	8	9	10	
DESIGNED BY:	BJV										

DRAWN BY: EJBF

CHECKED BY: DED

DATE: 02/14/2024

KIMLEY-HORN PROJECT NO.  
118019006

PHASING NOTES

SHEET NUMBER

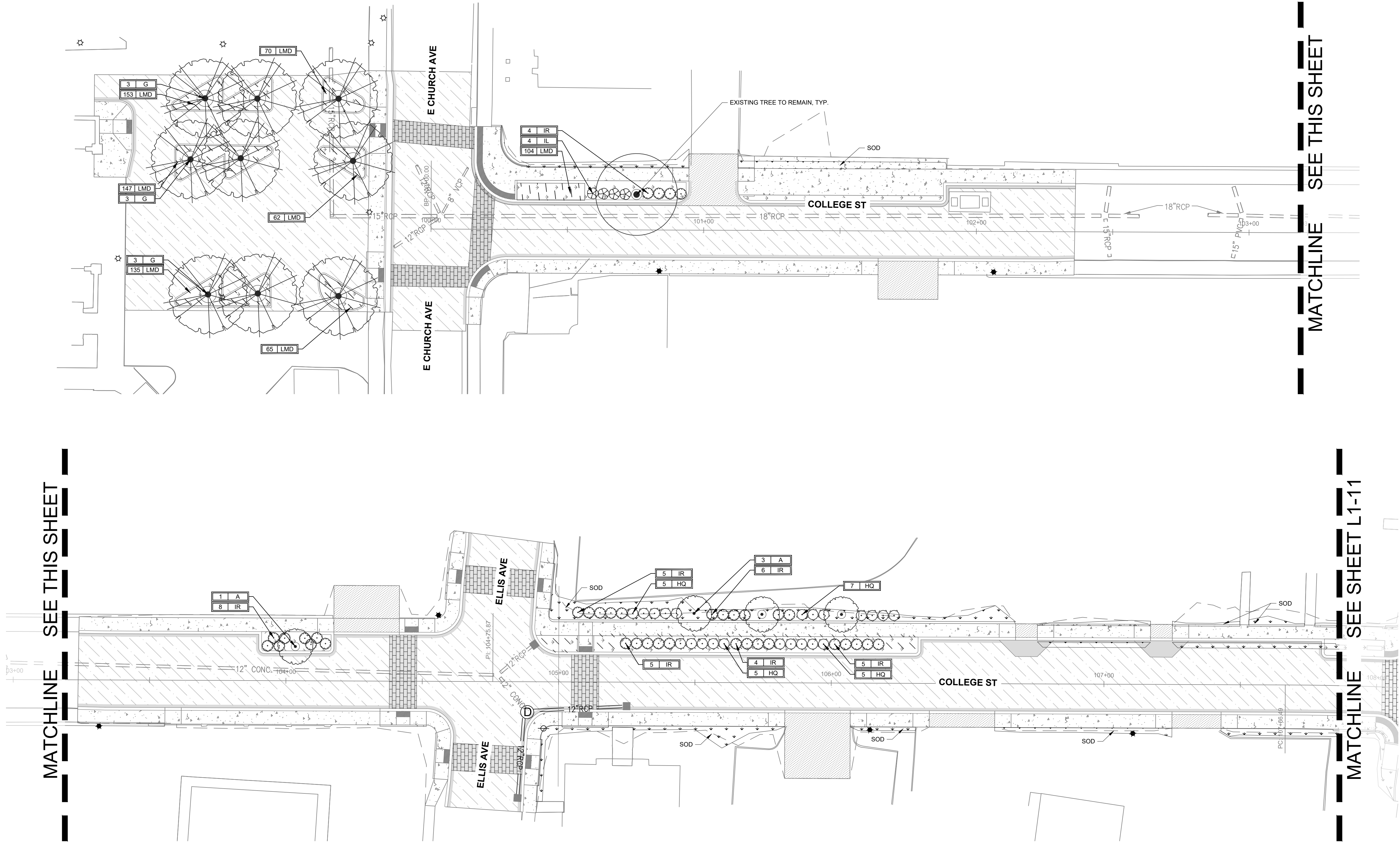
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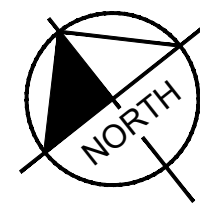
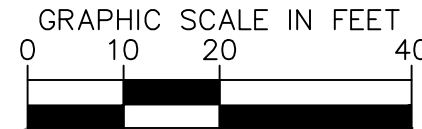




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LANDSCAPE PLAN NOTES	
1.	CONTRACTOR TO VERIFY UTILITY LOCATIONS BEFORE BEGINNING WORK.
PLANT SCHEDULE COLLEGE STREET	
CODE	COMMON NAME
TREES	
G	SKYLINE HONEY LOCUST
P	CUPRESSINA NORWAY SPRUCE
T	GREENSPIRE LITTLELEAF LINDEN
UNDERSTORY TREES	
A	'AUTUMN BRILLIANCE' SERVICEBERRY
SHRUBS	
DE	AUTUMN FERN
HP	LITTLE LIME HYDRANGEA
HQ	OAKLEAF HYDRANGEA
IL	VIRGINIA SWEETSPICE
IR	RED SPRITE WINTERBERRY
GRASSES	
CF	FEATHER REED GRASS
SI	LITTLE BLUESTEM GRASS
GROUND COVERS	
LMD	DENSIFLORA LILYTURF
TBS	TIFWAY 419 BERMUDA SOD



COLLEGE STREET LANDSCAPE IMPROVEMENTS  
CITY OF MARYVILLE  
MARYVILLE, TN

CITY of MARYVILLE  
PEOPLE are the KEY

REVISIONS		DATE	BY
No.	1	2	3
DESIGNED BY:	BPB		
DRAWN BY:	AMA		
CHECKED BY:	ARE		
DATE:	2/14/2024		
KIMLEY-HORN PROJECT NO. 118019006			
LANDSCAPE ENLARGEMENT			
SHEET NUMBER <b>L1-10</b>			

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MATCHLINE SEE SHEET L1-10



1. CONTRACTOR TO VERIFY UTILITY LOCATIONS BEFORE BEGINNING WORK.

<u>CODE</u>	<u>COMMON NAME</u>
-------------	--------------------

TREES	
G	SKYLINE HONEY LOCUST
P	CUPRESSINA NORWAY SPRUCE
T	GREENSPIRE LITTLELEAF LINDEN

## UNDERSTORY TREES

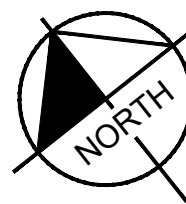
A	'AUTUMN BRILLIANCE' SERVICEBERRY
<u>SHRUBS</u>	
DE	AUTUMN FERN
HP	LITTLE LIME HYDRANGEA
HQ	OAKLEAF HYDRANGEA
IL	VIRGINIA SWEETSPIRE
IR	RED SPRITE WINTERBERRY

GRASSES

CF	FEATHER REED GRASS
SI	LITTLE BLUESTEM GRASS

### GROUND COVERS

LMD	DENSIFLORA LILYTURF
TBS	TIFWAY 419 BERMUDA SOD

[illegible]

DESIGNED BY: BPB

DRAWN BY: AMA

CHECKED BY: \_\_\_\_\_ ARE \_\_\_\_\_

DATE:	2/14/2024
KIMLEY-HORN PROJECT NO. 118019006	

LANDSCAPE  
ENLARGEMENT

SHEET NUMBER

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